

City of Spokane

Downtown Design Guidelines

**Recommended for approval by
the City of Spokane Plan Commission
at a Public Hearing held on
April 12, 2000**

**Approved by the
City Council
On October 30, 2000**

**Implementing the Downtown Plan
Charting the Future: The Plan for a New Downtown,
adopted by the City Council
on March 29, 1999**

CITY OF SPOKANE

Downtown Design Guidelines

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City of Spokane

Downtown Design Guidelines

I. INTRODUCTION

The Spokane City Council recently adopted a strategic plan for the downtown area of the City – *Charting the Future: The Plan for a New Downtown*. The City Council directed that a zoning ordinance amendment and design guidelines be prepared to implement the *Plan*.

DESIGN GUIDELINES & DESIGN REVIEW PURPOSE

Establishing Downtown Design Guidelines is essential to improving quality of life, economic vitality, and a positive visual image for the City. These design guidelines are meant to encourage greater variety and creativity in the design of elements that together make up a development proposal. These elements include site design and organization, building design, and landscaping. The guidelines are intended to be flexible, practical, performance based, and an effective means of creating compatibility in the environment in building form, architectural treatment and overall function.

The design guidelines are not requirements, but rather, suggestions for innovative design solutions. They describe design alternatives that have contributed positively to other successful urban downtown environments. The guidelines are a collection of ideas for making great places – they are concerned with the social fabric of urban environments, how people use spaces, and how to create an active, unique, and attractive downtown. The guidelines are intended to be educational, and are by no means comprehensive. As we continue to search for creative design solutions to downtown issues, we welcome feedback from planners, architects, landscape architects, and designers, as well as from the public.

The intent of Design Review in Downtown Spokane is to achieve a better Downtown and to improve the quality of life for all Spokane residents through attention to simple design principles in new development. Design review, together with zoning regulations, offer the flexibility that enables a development proposal to better respond to site specific conditions and surrounding restraints, conditions and character. Design Review is not intended to take the place of zoning as a means to regulate land use, unless specifically allowed in the zoning code as a means of altering adopted density and development standards.

The general purpose of Design Review and the Design Review Guidelines is to:

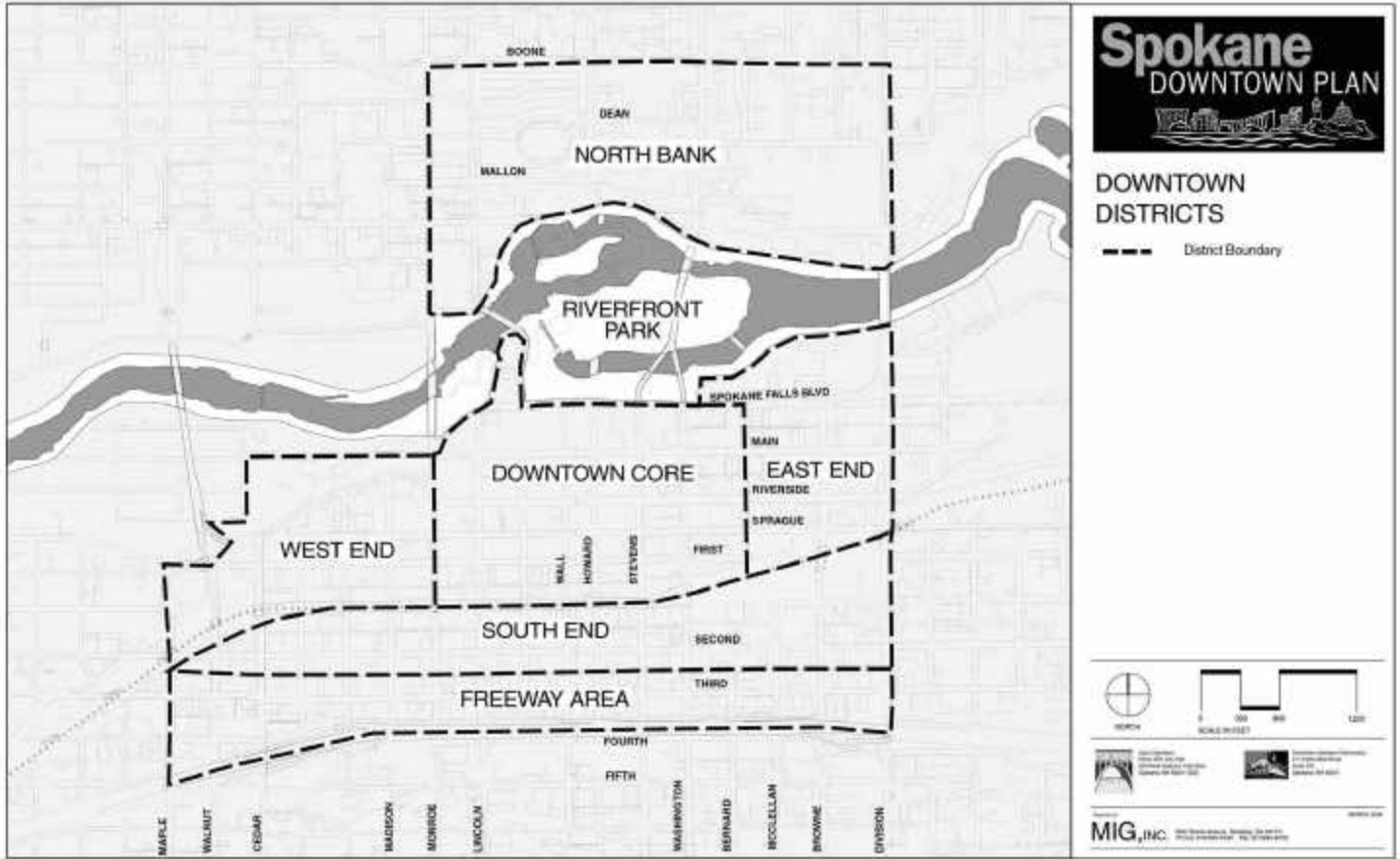
- Implement the Spokane Downtown Plan
- Ensure that community values, expressed through public participation, regarding built and natural environments are embodied in new developments;
- Foster attractive and functional design solutions in the community as a place to live, work and recreate;
- Preserve the character and quality of the Downtown by maintaining the integrity of those areas that have special character and or significance;
- Protect public and private investments in the Downtown;
- Provide design direction to applicants, designers and decision makers, while not substantially adding to the development permitting process time frame, and;
- Provide a forum for the exchange of design ideas and approaches so that a flexible application of design guidelines and standards may be permitted as a refinement of established standards.

The Design Guidelines are also intended to assist in conservation and revitalization of existing properties in the Downtown. They describe and illustrate ways existing buildings and storefronts may be sympathetically altered and improved, and ways in which new developments may be designed and constructed to achieve compatibility with the urban character of Spokane.

The guidelines are also intended to facilitate an expeditious project review process for applicants, helping them to anticipate major design issues and proposing design solutions at the earliest possible stage in a project's development.

DESIGN STANDARDS AND DESIGN CRITERIA

While the design guidelines present creative suggestions for meeting Downtown design objectives, design standards and design criteria are intended to serve as a more rigorous, quantitative means of implementing design objectives. Design criteria are specific performance measures, rules, or tests upon which a judgement, decision or recommendation can be based regarding the fulfillment of the design objectives. Standards are written in quantitative measures, as minimum or maximum levels of attainment. *For example*, a standard for a deck or balcony for residential units in multi-family buildings should be a minimum of 6' feet wide. Design standards and design criteria are **not** covered in this Design Guidelines document, but are addressed in the Zoning Ordinance.



THE DOWNTOWN AREA/DISTRICTS

The downtown design guidelines apply to development within the six CBD zoning districts. The Core is the high intensity, pedestrian-oriented center of the city with many offices and street level commercial activities. The East and West Ends are mixed-use areas where residential and community-serving retail uses are encouraged, and where warehousing and light industrial uses are still located. The South End is a mixed-use area where uses including residential to light industrial are located and permitted. The Freeway Area is an auto-oriented commercial area typified by fast food establishments and service stations, where residential uses are not permitted. The North Bank, located immediately north of the river, is a mixed-use area of residential, commercial, recreation, and entertainment uses.

DESIGN REVIEW PROCESS

The design review process typically begins at the initial contact between a project proponent and the city staff. This usually happens at a pre-development meeting where the proponent describes what the expectations are for the particular project and the staff describes the approval process, identifies potential issues and answers questions. At the pre-development meeting, required for projects in the downtown, the proponent will be made aware of, and presented with, the downtown guidelines. Since the pre-development meeting usually occurs early in the design process, the guidelines can serve their purpose of educating the designers of the desires of the community, and be incorporated into the design parameters of the project.

A project would go before the Design Review Committee only if one of the following tests is met:

- The project is one of the types specified in Spokane Municipal Code Chapter 4.13 as requiring design review,
- The project is located along a designated Type I green street,
- The project is more than 60 feet high and within 300 feet of a designated downtown landmark tower,
- The project is a surface parking lot, or
- The proponent desires an exception to the downtown zoning standards via the Plans-in-Lieu process.

If it is decided that a project is to be reviewed by the Design Review Committee, the process and time frame are as follows:

- An application, fee, and appropriate drawings, usually site plan, landscape plan, floor plan and elevations are submitted to the Planning Department. This can be done separate from, or in conjunction with, other discretionary review processes.

- The deadline for complete submittal of application and information is 21 days prior to the workshop at which the review of the proposal is desired. During this time the staff will review the project, write a staff report and schedule a workshop with the Design Review Committee. The staff report will generally be available one week prior to the meeting.
- At the meeting the staff will present the staff report and the proponent will be given an opportunity to present the proposal and address staff concerns.
- Although the workshop is open to the public, testimony is generally not taken since the intent of the workshop is to discuss, and come to an agreement on how to best address the issues identified in the appropriate adopted plans and guidelines.
- After discussion, and depending upon the complexity of, and the adequacy of the submitted materials to accurately portray the proposal, the Design Review Committee will make a recommendation regarding the project.
- Within 7 days, the staff will prepare a written summary of the recommendation and recommended conditions, if any, to the appropriate discretionary city official or board.

HOW TO USE THE DESIGN GUIDELINES

The Downtown Design Guidelines are for the use of the participants in downtown development. The guidelines help the property owner, project designer, and developer identify community preferred design objectives and various optional methodologies to attain the objectives. They also serve to provide citizens and other property owners with a certain degree of certainty about how a future project will look, function, and what impact it may have on their property and investment.

The City staff use the guidelines to educate the public, the project proponents and city officials about the preferred design objectives, and as a tool for project evaluation and preparation of staff reports. The guidelines give the Design Review Committee an evaluation tool, and focal points for discussion with the project proponents. They also provide the basis for project recommendations to City officials. The checklist at the end of the guidelines provides the proponents, the City staff and the Design Review Committee a quick reference tool to use during the design development and review processes to help make sure that all the important issues are addressed.

DESIGN GUIDELINE FRAMEWORK

The following is an organizational framework of design guideline topic areas and definitions of terms. The guidelines describe both the intent and objective for each topic area, and include diagrams, sketches, and/or photographs to illustrate alternatives for applying guidelines.

Topic/Title

Design Objective

This section identifies the purpose and objective to be achieved or accomplished for this topic area.

Discussion

This section discusses the intent and describes why this guideline is important to the quality of life in the Downtown. The discussion may also relate facts and evidence to support the design guideline objective.

Design Guideline

This is a written statement of desired performance that establishes a qualitative, as opposed to quantitative, level of design attainment to meet the design objective. The design guideline proposes possible design solutions as ways of accomplishing design objectives.

Concept Diagram, Sketches and Photos

Guidelines are accompanied by concept diagrams, sketches, or photographs. These images are illustrative in nature, and are intended to demonstrate one way the design objective could be achieved.

II. GUIDELINES

A. Site Design and Layout

A.1 Building Setbacks

Design Objective

Building setbacks help define street and sidewalk areas as active public spaces. Historically, commercial buildings in the Downtown have been built to the front property line, behind the sidewalk. This “street wall” shall be retained and reinforced in new construction and additions.

Discussion

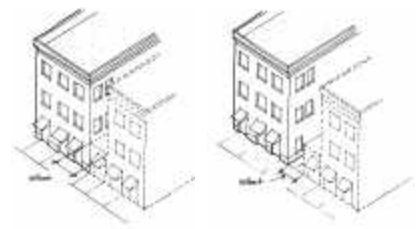
Downtown buildings have historically been built to cover the entire lot, with no front, side or rear setbacks. This type of site design helps define Spokane’s Downtown character. Parking is typically provided within the building, below grade, or in parking structures.

Guidelines

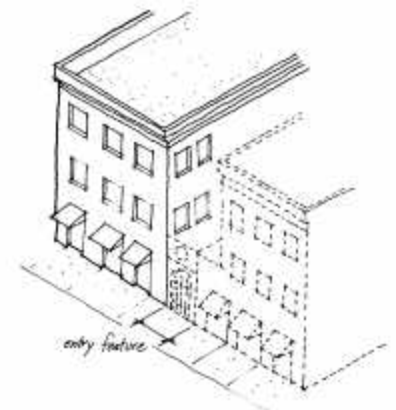
- A.1.1 Constructing buildings to the back of sidewalks, along the street, and from side property line to side property line reinforces the vitality of the public sidewalk.
- A.1.2 Locating building entrances close to the street helps to maintain visual surveillance of the street and sidewalk areas.
- A.1.3 The historic angled “keyhole” entryways found in many Downtown commercial storefronts contribute to the unique ambiance of Downtown Spokane.
- A.1.4 New construction should take into consideration older buildings with windows on, or adjacent to, side and rear property lines. Providing an appropriate setback to new buildings adjacent to existing older buildings with windows facing rear and side-yards allows for light, air and usable space between the buildings.



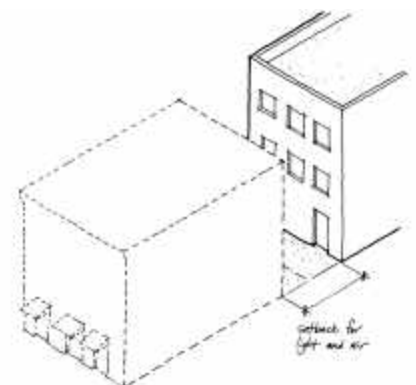
Historic keyhole entryways contribute to the unique ambiance of Downtown.



Allowing side setbacks for new buildings adjacent to existing side windows preserves visual access.



Side setbacks can provide opportunities for entry features, private patios, or service access.



Providing rear setbacks for new buildings allows light and air to penetrate.

A.2 Outdoor Areas

Design Objective

All site spaces shall be improved for uses and activities to reduce vandalism, increase safety and to provide more attractive and functional spaces.

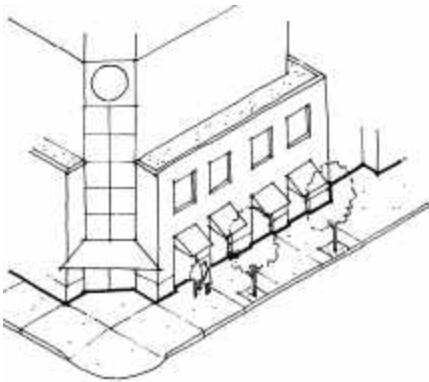
Discussion

Defensible space is space that, by virtue of design, contributes to personal safety. Sites should be designed with attention to visual surveillance, lighting and safe circulation through and around spaces. The rear portions of many commercial buildings are often facing alleyways and are frequently underutilized and unattractive. These underutilized spaces are often dark, unsafe, and collect unwanted trash and garbage.

Rear-lot areas of commercial buildings in the Downtown provide an opportunity for service access, deliveries, proper trash collection enclosures, and additional parking. In some cases, these rear-yard areas also provide opportunities for creating attractive outdoor areas for restaurants, gardens and patios spaces.

Guidelines

- A.2.1 Controlled access points, good lighting, glazing, cut-away corners, and maintaining visual surveillance over spaces are important aspects of providing “defensible space.” Designing sites so that people entering and exiting can be easily observed helps create a safe, defensible environment.
- A.2.2 Using rear-yard areas and alleyways for service access and maintenance helps preserve pedestrian-friendly public street fronts. Well-lighted rear-yard areas and alleyways contribute to overall safety and security.
- A.2.3 Outdoor dining areas, patios, and gardens help create a vibrant pedestrian environment. Small areas for public plazas, patios for seating (tables and chairs) placed on the public sidewalk immediately adjacent to an indoor café or restaurant invites pedestrian activity.



Defensible space is created with controlled access points, cut-away corners, and optimal visual surveillance over an area.



Rear lot areas provide an opportunity for patios and courtyards.



Outdoor dining areas and patios help create a vibrant pedestrian environment.

A.3 Building Heights

Design Objective

The tallest building heights shall be concentrated in the Downtown core with decreasing size and intensity as one moves out, away from the Downtown.

Building heights shall be reduced closest to the Spokane River to provide views of River Front Park from buildings farther from the River's edge.



Building heights shall be reduced closest to the Spokane River to provide views of River Front Park.

Discussion

Building heights in the Downtown vary from district to district. In all cases however, buildings are historically over one story in height (minimum 28'), and in most cases buildings are four to six stories in height. In the central core many of the buildings are 12 to 15 stories in height.

Taller buildings that shape the skyline and create a strong visual landmark for the region are encouraged. Tall buildings are the hallmark of an urban downtown city of the size and stature of Spokane.

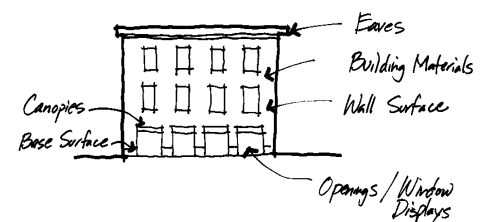
Corner sites pose a special opportunity for providing additional building height. Corner buildings can serve as anchors for Downtown blocks and can be taller and more elaborate than mid-block structures.

Guidelines

The Downtown Core, East and West End Districts

The Downtown Core area is intended to reinforce the "urban" character of the street and to provide for an active, pedestrian-friendly environment that encourages walking and outdoor street life.

A.3.1 Maintaining the alignment of building cornices, rooflines and building lines of new buildings adjacent to existing buildings preserves architectural continuity. Building heights of new buildings can reinforce traditional building facades by falling within the range of the building heights found on the immediate block or in the surrounding district. Because heights in many of the Downtown districts vary, not all buildings on a block are appropriate for matching building heights.



New buildings can maintain block character by using architectural features of existing buildings.

A.3.2 Taller buildings can reinforce the traditional building street face along a block, or within a district, by matching cornice



Matching cornice lines of new buildings to the traditional street façade helps to maintain an architectural continuity..

lines of adjacent buildings and by stepping back upper floors above average building heights.

- A.3.3 Matching building heights at the ends of blocks on adjoining corners can help create a unified architectural character.

In the Freeway Area

The Freeway Area is intended to serve as an area for freeway-oriented uses and buildings. These could include fast food restaurants, auto dealers and auto parts stores, gas stations and other freeway-oriented activities.

- A.3.4 The Freeway Area allows for all types of building design and building heights, including one-story freeway-oriented commercial buildings, such as national franchise designs oriented to automobile users.

- A.3.5 Building heights need not be consistent with surrounding building heights.

A.4 Parking Lot Placement and Design

Design Objective

Parking lots shall be designed so that they are accessible, but do not unnecessarily intrude upon the urban character and the pedestrian quality of the Downtown.

Discussion

Parking in the Downtown is important to the success of Downtown businesses. However, surface parking lots destroy the pedestrian character of a Downtown. Surface parking lots preclude retail activity on the street and detract from the Downtown as a lively and attractive place to gather. Too often, surface parking lots create the appearance of a vacant, underutilized, and unsafe downtown, affecting the commercial viability of existing retail establishments and deterring visitors and shoppers from using the Downtown.

Guidelines

- A.4.1 Siting large expanses of paved surface parking lots in front of buildings increases walking distances to destinations, precludes retail activity on the street, and detracts from the attractive Downtown ambiance.
- A.4.2 Siting Downtown parking lots to the rear of building sites, away from major pedestrian commercial streets with access from side streets and alleyways, supports an active, pedestrian street environment.
- A.4.3 Perimeter landscaping can help screen cars from public view along public sidewalks and pedestrian thoroughfares. Landscaping also serves to soften the edges of expansive, paved surface parking lots.
- A.4.4 Designing parking into parking garages below or above street level retail or commercial uses, allows for pedestrian activity along the street while providing parking convenient to Downtown destinations.
- A.4.5 Fences, walls, and/or landscaping can help to screen commercial properties with surface parking lots adjacent to public sidewalks. Opaque screening materials can completely block views from the street into parking lots, making surveillance difficult. Using semi-transparent screening materials and adjusting wall/fence/planting heights appropriately can aid in maintaining sufficient visual access



Siting large, exposed surface parking lots to the rear of buildings preserves the pedestrian street environment.



In this photo, perimeter landscaping could have helped screen cars from public view along the pedestrian thoroughfare.



Decorative fences around parking areas that allow visibility into the site are encouraged

into sites for safety, while also providing attractive design features.



Screening surface parking lots with landscaping is encouraged



Decorative walls with landscaped amenities can screen unattractive surface parking lots from public walkways.

- A.4.6 Decorative fences or walls adjacent to the sidewalk can screen unsightly views of parked cars. A narrow landscape buffer strip placed in front of fences/walls provides an attractive barrier. Automatic irrigation systems help to ensure buffer strip maintenance.
- A.4.7 Landscaped berms are not in keeping with the urban Downtown character.
- A.4.8 Use of a trellis-type structure situated above a wall or fence can be provided to help visually maintain the street wall and improve the pedestrian environment along the street.
- A.4.9 Seating, lighting, trash receptacles, telephones, and other pedestrian amenities deigned into the screening wall and landscaped setback areas contribute to a comfortable and attractive pedestrian environment.
- A.4.10 Parking lots in the Downtown can also be improved with appropriate signage and well designed ingress and egress points that reduce conflicts with the pedestrian movement.

B. Transportation & Circulation

Streets

A basic goal of the Downtown Plan and these Design Guidelines is to enhance the livability of the streets in the Downtown area. The streets should provide an environment that supports independence and freedom of choice; provides orientation, safety, and comfort; encourages a sense of community and place; foster a sense of neighborly ownership and responsibility; avoid disturbing nuisances; and enhances the economic value of adjacent properties.

Streets are a vital part of every day operations of the Downtown. The street system provides access to all the necessary activities and uses in the Downtown as a place to work, shop, live and recreate. Street design must accommodate the movement of good and people by all modes of travel; autos, bus, pedestrian, bicycles and potentially future light rail services.

A major difference between urban streets and other streets in the regional network is the quality of the pedestrian environment along the streets. The urban streets are designed to support public social interaction and enhance the pedestrian experience between the building and the travel lanes in the street. The major objectives of the design of Downtown Streets are to:

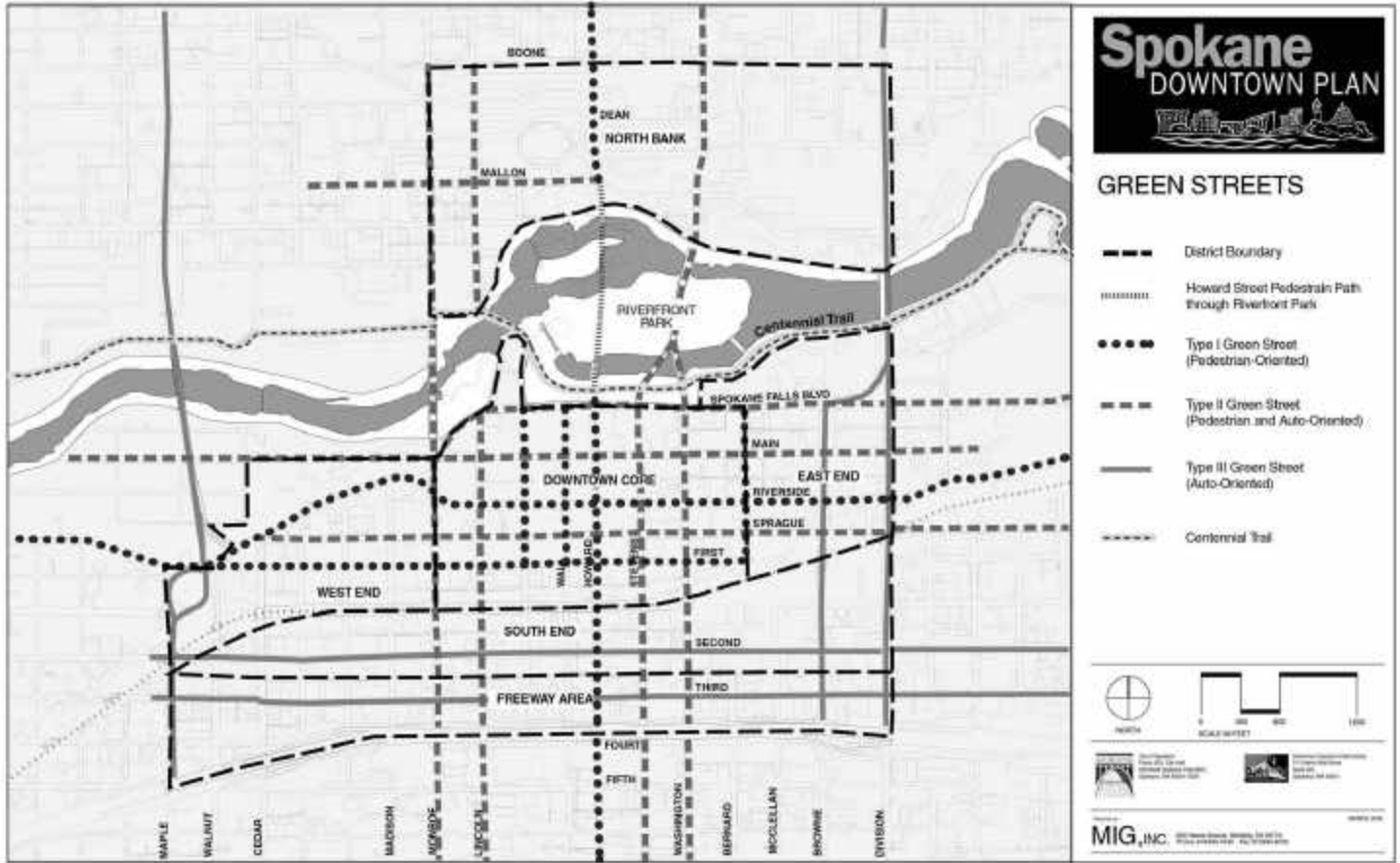
- Provide orientation and identity to the Downtown,
- Provide a safe environment,
- Provide for physical comfort,
- Strengthen the spatial definition of the street by orientation of buildings to the street,
- Provide high quality design and construction,
- Maintain and enhance the quality of the environment, and to
- Facilitate alternative modes of travel, auto, pedestrians, transit, and bicycle.

Green Streets

Green streets are well-landscaped, tree-lined corridors designed for both pedestrian and vehicular use. Green streets included three street functions major landscaped pedestrian streets, boulevards and landscaped arterial streets. The concept of Green Streets in the City of Spokane is based on the parks and recreation plan developed by the famous Fredrick Law Olmstead design firm in 1913. The original Green Streets concept identified major streets to connect a system of parks and open spaces through the City of Spokane. Pedestrian amenities on Green Streets may include street furniture, decorative lighting, wide sidewalks with extension (bulb-outs) at street corners, decorative crosswalks, art, outdoor restaurants, plazas and improved



Streets are a vital part of the Downtown, providing access to places for working, shopping, living, and recreation.



sidewalk building interfaces e.g., awnings, street-oriented retail activity). Use of amenities must conform to the City of Spokane's sidewalk encroachment guidelines (see appendix).

Three types of green streets have been identified in the City's Downtown Plan, based on the primary function of each street within the Downtown network.

Type I

Type I green streets are slow, pedestrian oriented streets. Type I Green Streets are typically two-way streets with wide, well maintained sidewalks and pedestrian amenities to encourage strolling, walking and shopping at the street level. Type I Green Streets provide pedestrian connections within the Downtown and surrounding neighborhoods.

Type I Green Streets in Downtown include:

- First Avenue
- Bernard Street
- Howard Street
- Post Street
- Riverside Avenue and
- Wall Street

Type II

Type II Green Streets are major boulevards and avenues oriented to both the pedestrian and the motorist. Type II Green Streets move pedestrian and the motorist into and around Downtown. These streets also provide major pedestrian connections to surrounding neighborhoods and districts, park and open spaces.

Type II Green Streets in Downtown Spokane include:

- Lincoln Street
- Main Avenue
- Mallon Avenue
- Monroe Street
- Spokane Falls Boulevard
- Sprague Avenue
- Stevens Street, and
- Washington Street.

Type III

The primary function of Type III Green Streets is to move the motorist through Downtown. These are intended as attractive, well-landscaped arterial boulevards. Type III Green Streets will be improved overtime with additional street trees and landscaping, with

adequate sidewalk widths and design amenities to provide of safe and attractive pedestrian circulation through the Downtown.

Type III Green Streets in Downtown Spokane include:

- Division Street
- Browne Street
- Maple Street
- Second Avenue and
- Third Avenue.

B.1 Type I: Pedestrian-Oriented Streets

Design Objective

To provide primarily a safe and attractive pedestrian oriented environment for walking, strolling and shopping. Auto traffic on these streets allows for access to adjoining uses and parking.

Guidelines

- B.1.1 Street activity can be enhanced by creating “active street edges” such as windows and entrances opening on to the street, outdoor street displays and sales, and street cafes and outdoor restaurant seating.
- B.1.2 Street furniture such as fountains, benches, can invite pause and make the walk an extremely pleasant experience.
- B.1.3 To make the street ‘places’ it is necessary for them to have a strong physical spatial definition. This can be achieved by having an appropriate proportion of street width and vertical defining elements such as buildings or trees or both.
- B.1.4 An appropriate proportion between street width and the vertical defining elements also helps in making them comfortable places in terms of climatic considerations. Comfortable streets offer warmth or sunlight when it is cool and shade and coolness when it is hot.



Make the streetfront active with windows and entrances on the street



Provide active, pedestrian-oriented ground-level uses at regular intervals along the street

B.2 Type II: Pedestrian and Auto Serving Streets

Design Objective

Type II Green Streets shall provide a street experience that is enjoyable to both pedestrian and auto-traffic. They shall also be well connected and easily accessible to surrounding districts.

Guidelines

- B.2.1 Creating a realm of pedestrian safety as well comfort and ease of access will make the streets an enjoyable experience. Pedestrian comfort and safety can be achieved by creating an appropriate width of sidewalk and adequate buffering from traffic. A continuous row of trees close to the edge of sidewalk offers a sense of safety and tranquility to the pedestrian.
- B.2.2 For the streets to be well used by pedestrians, they need to be served by convenient pedestrian connections to transit and between land uses and transit facilities.
- B.2.3 Orienting land uses to the street will increase and support pedestrian activity while also making it an informative and visually interesting for the auto user.



Street trees help create a pedestrian friendly environment

B.3 Type III: Auto-Oriented Boulevards

Design Objective

Type III Green Streets shall provide for a safe and attractive landscaped boulevard environment from moving traffic through downtown to major destinations. Landscaped boulevards through the downtown shall have a distinctive “urban” character.

Guidelines

- B.3.1 Street circulation and access can be simplified and the visual appearance improved by consolidating driveways and parking entries where possible and using shared entry and exit points, through use of common access easements. Consolidating entries reduces the number of potential conflicts with pedestrians and the travel lanes in the street.

- B.3.2 Reducing the number of signs on buildings and a site will help to create a more attractive image along the boulevard, reduce visual clutter, and create a more consistent image and character in the Downtown.
- B.3.3 Landscaping and street trees along the sidewalks, between parking and moving lanes in the street and the building edge, help define the pedestrian zone and help to create a safer pedestrian walk-way along the boulevard.
- B.3.4 Reducing the number of pole signs and use building mounted and monument signs along the street can help to create a more attractive boulevard, reduce visual clutter, and create a more urban experience.
- B.3.5 Use of attractive street lighting and pedestrian amenities along the street (i.e. benches, trashcans, newspaper boxes, etc.) using a similar design or “family” of fixture styles helps to create a coherent and consistent character and design image for the boulevard.

B.4 Pedestrian Circulation

Design Objective

The design of pedestrian circulation shall create a functional, safe environment for the pedestrian that provides a continuous travel corridor for pedestrians, serving the same major destinations as automobiles. The pedestrian circulation serves local land uses by providing pedestrian access to commercial and residential buildings; serves transit and transit facilities; provides open space and public outdoor activity space to the city supporting social contact and interaction, and provides a buffer from the traffic and noise of the street.

Discussion

Establishing an active and vital pedestrian environment at the street level is critical the function of the Downtown as an urban economic center of the region. The pedestrian circulation system requires attention to safety as well as comfort and ease of access. Pedestrian safety and comfort are directly related to the width of the sidewalk and the buffers created from the travel lanes in the street.

The pedestrian environment along the street is also formed by the adjacent buildings and landscaping. Providing a strong spatial definition along the street with building walls and tree canopies add to the distinct urban character of Downtown, helps to create a “sense of place” enhancing the status of the street and adjacent property values.

Guidelines

B.4.1 Adequate width for sidewalk uses is critical for people moving along the street, lading and unloading, on-street parking, walking, window shopping, bicycle parking and the use of street furniture. Think of the sidewalk as divided into separate functional zones. Wider sidewalks (over 12') can accommodate more intensive pedestrian traffic and uses on the sidewalk by local merchants and residents.



Appropriate widths for sidewalks must be provided to allow for walking, window-shopping, outdoor dining, and other uses.

B.4.2 Mid-block pedestrian crosswalks can be added where blocks are too long to reasonably expect pedestrian to use corner crosswalks. Mid-block crosswalks should be added only where factors such as street width, traffic speed and sight lines allow for safe pedestrian crossings.

B.4.3 Use of special, decorative paving treatments can help to separate the pedestrian zone from the street travelways at intersection crossings.

B.4.4 A continuous sidewalk improvements along major arterial streets insures safe pedestrian connections. Where ever possible new projects and renovation of exiting sites should close the gaps between pedestrian connections by providing pedestrian and sidewalk improvements on all streets.

B.4.5 In Downtown areas the width of sidewalks should be a minimum of 12 feet. Wider sidewalks (15-20 feet in width) are better along major commercial streets. Wider sidewalks provide enough room for two people walking together can pass one another without making abrupt changes in direction, provide space for pedestrian amenities closer to the street travel lanes, an allow for outdoor eating and displays).

B.4.6 Bulb-out at street corners help to reduce the travel time crossing streets and provide a safe zone for the pedestrian at intersections and provide opportunities for additional street furniture, landscaping, seating and pedestrian oriented signs.

B.4.7 Street trees, and pedestrian furniture located between the parking and travel lanes in the street and the sidewalk increases creates a buffer zone for the safety of the pedestrian and creates a more comfortable pedestrian experience along the street. However, the placement of street furniture, utility poles, parking meters, signs, and street trees should be spaced to not obstruct movement from the parking space to the building entry, or prevent car doors from opening at the sidewalk edge.



Street trees between the street and sidewalk create a buffer zone for a comfortable pedestrian experience.

- B.4.8 In general, sidewalks and bikeways should be separate to provide ease of movement and safety of the use, unless designed as a specific multi-use path separated from the street.

B.5 On-Street Parking

Design Objective

On-street parking improves the safety of pedestrians on the sidewalk and provides access to adjoining activities and uses. On-street parking shall be designed to be safe and convenient for users and fit the level of activity on adjacent properties and the level of traffic in the street.

Discussion

On-street parking helps to create an active and vital street life. It provides additional parking and access to adjoining commercial and residential uses, and also provides a buffer zone between the pedestrian sidewalk and the travel lanes in the street. On-street parking also decrease the capacity of the adjacent travel lanes between 3% and 30% depending on the number of travel lanes, the width of the adjacent travel lanes, and the frequency of parking maneuvers. Thus, through traffic and local access requirements must be balanced when deciding on where to provide on-street parking. The Downtown plan identifies streets as major through corridors and local serving streets.

Guidelines

- B.5.1 On-street parking provides a buffer between the pedestrian way and the moving vehicles on streets and boulevards.
- B.5.2 On-street parking provides access to adjoining businesses, residential uses, and other land use activities in the Downtown. At the same time on-street parking increases the activity and vitality of the street environment and provides for passenger and freight loading and unloading areas.
- B.5.3 On slower, pedestrian oriented streets local commercial streets, on-street parking can be provided with angled parking to increase the number of parking spaces. Angled parking should maintain the functional level of auto circulation in the street.
- B.5.4 Parallel parking works better on major regional arterial streets with major through traffic (Type III Green Streets). Local

commercial streets (Type I Green Streets) can accommodate a variety of on street parking configurations based on the width of the street.

- B.5.5 On street parking lanes on slow, pedestrian oriented shopping streets can also serve as outdoor eating and retail displays areas during special events, or special evening hours. Cafes and restaurants can use the on-street parking zone on a limited basis during the evenings to encourage out door activity and social interaction. Use of on-street parking requires special parking limits and enforcement procedures.

B.6 Bicycles

Design Objective

Accommodations for bicycles and bicycle paths shall be designed to create safe, active and an attractive environment throughout Downtown. The design of bicycle circulation shall create a functional, safe environment for the bicyclist that provides a continuous travel corridor serving the same major destinations as automobiles. The bicycle circulation system shall serve local land uses by providing connections and access to commercial and residential buildings; transit and transit facilities; is part of the City wide open space and public outdoor recreational activity space, and provides an alternative mode of transportation to the automobile.



The design of bicycle circulation shall create a functional, safe environment for bicyclists.

Discussion

Bicycle circulation can be provided through a number of alternative facilities, bicycle lanes, bicycle routes and bicycle paths. In addition, bicycles share the roadway network using the street travel. Bicycle lanes are the preferred bikeway design choice for throughway (highway and arterial streets), boulevard road classifications. A bicycle lane is a portion of the roadway designated for exclusive or preferential use by bicyclists. Some general design considerations and design guidelines are described below.

Guidelines

The Downtown is greatly enhanced with facilities for bicyclist; bike racks, bike lanes and signage for wayfinding.

- B.6.1 Security bicycle parking on development sites and at transit stops encourages bicycle use and convenience. Bicycle parking on sidewalks or on-street, in lieu of an auto parking stall can be placed close to major facilities.
- B.6.2 Bicycle lanes should be well maintained and clean to ensure a smooth, unobstructed travel-way. Pavement conditions

should ensure a smooth clean travel-way by eliminating height difference between gutter pans and asphalt, driveway curb cuts and the travel lane, and other irregularities in the pavement surfaces.

- B.6.3 Bicycle lanes and routes are designed as one-way systems in the same direction of travel as vehicles. An exception is on one-way streets which can allow for opposite direction bicycle lanes separated from travel lanes (contra-flow lane) with an appropriate safety barrier or separation.
- B.6.4 Bicycle lanes and facilities should use local standards for bike design.
- B.6.5 Signage and pavement marking should be provided along the entire length of bike paths in the Downtown.
- B.6.6 Bike lanes should avoid streets with diagonal parking.

B.7 Gateways and Entryways

Design Objective

Gateways and entryways shall be designed to announce the transition and arrival into Downtown Spokane. These visual features are civic in emphasis and serve to identify and promote the distinct identity of Downtown Spokane.

Discussion

Gateways and entryways area should assist and enhance the visitors' experience when entering into the Downtown area. These features serve as landmarks and should be visible to vehicular, bicycle, and pedestrian traffic.

Gateways and entryways should be designed to create a high quality visual environment for the public. These sites can provide an opportunity for architectural features, monuments, public art, signage, flowers, trees, and other landscaping.

Guidelines

- B.7.1 Gateways and entryways should be located at major access routes into Downtown Spokane.
- B.7.2 Signage should be civic in emphasis and announce the entrance into Downtown Spokane. Commercial and tenant names should be absent.
- B.7.3. Gateway features should be cohesive in nature and integrated with their surroundings.
- B.7.4. Gateways and entryways should be illuminated at night. Architectural features should be built to a height visible from vehicles.

C. Building Design

Building design guidelines for Downtown Spokane primarily address the exterior of buildings and the relationship of buildings to the surrounding setting or context and the street. While building design decisions must balance many factors including economic constraints, programmatic needs, functional requirements, and aesthetics, to name a few, the relationship of the building to its downtown urban setting is the primary issue of public concern. The following building design guidelines address those public issues of site and street relationships.

Major design principles

There are two major design principles that are paramount to building design in Downtown Spokane. The first is the principle of “*contextual fit*” – how well does the proposed building “fit” within the Spokane downtown urban setting. The second major principle is “*pedestrian friendly streets*” – how does the building design contribute creating an active, pedestrian street life.

Contextual fit

Contextual fit requires evaluating the existing buildings on the block and in the surrounding district to determine the major reoccurring design elements that contribute to the character and image of Downtown as an urban place. These design elements of contextual fit include features such as building setbacks, building heights, building form, rhythm of openings, rhythm of horizontal building lines, color, materials, texture, building style, and building details. Historically, over time, a pattern of repeated design elements contribute to the overall character and image of Downtown Spokane.

A new building proposal need not match every building element to “fit” within the context. The more elements a new building design addresses, however, the more likely the design will contribute to the existing contextual pattern of Downtown.

In some cases, on some sites, the opposite design principle may be appropriate - creating a landmark or signature building. A signature building design creates a building that is the opposite of “contextual fit.” Signature buildings stand out in the urban setting because of their unusual design character. Such buildings are often designed as new “cutting edge” building styles or experiments in architectural design by a leading architectural designer.

Creating many “signature” building designs within one district creates visual confusion and clutter. Thus, it is important to determine when and where a “signature” building design would be appropriate in the Downtown. Many signature buildings are created for public or civic

use, such as museums, government centers, schools, churches, and major recreation facilities. Some appropriate sites for landmark or signature buildings in the Downtown area include the North Bank commercial recreation area, the Summit Site, and the Higher Education Research District. These sites have relatively few older buildings that form a consistent historical context, and provide larger open sites for major new buildings. In particular, the types of uses in the North Bank commercial/recreation area provide opportunities for the creation of major landmarks and can help to draw visitors from the Downtown Core to those activities.

In most cases the principle of “contextual fit” is appropriate for building design in Downtown Spokane. Building designs that would create a signature building should provide a more detailed analysis of the site and district and how the proposed building would be appropriate for the site.

Pedestrian-friendly streets

The second major principle for building design is the creation of a “*pedestrian friendly*” urban street environment. The types of building design elements that contribute to a pedestrian-friendly street environment include: street-level activities, building to the edge of sidewalks, windows and openings at the ground floor, awnings and canopies over window displays and entries, pedestrian amenities along the street, and extending building activities into the sidewalks such as outdoor seating, dining and sales displays.

Buildings that are designed as signature or landmark buildings can also meet the second principle of creating a pedestrian friendly street environment. New and innovative building designs should also be pedestrian friendly, inviting, and contribute to the Downtown as a lively and active place.

C.1 Proportion of Openings

Design Objective

The proportion and rhythm of building openings shall be maintained to create a consistent urban image and character in Downtown Spokane.

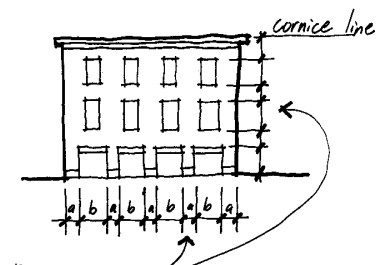
Discussion

Building openings, windows, doorways, and entries contribute to the consistent urban character and image of Downtown Spokane. A common element is the similar size, spacing and shape of window openings. Older buildings that are primarily two stories or more have predominantly narrow, vertical orientation, and are stacked above each other with a regular spacing. New, modern style buildings, however, often use continuous “ribbons” of glass that are horizontal in form and wrap the building with no spacing between openings.

While every building in the Downtown need not have the same window design treatment, repeating the pattern of historic window openings helps to reinforce the character of Downtown as different from other suburban centers in the Spokane region.

Guidelines

- C.1.1 Maintaining building widths and historic proportions and spacing of building openings (windows and entries) preserves the architectural rhythm of Downtown street facades. Openings in the facades of Spokane Downtown buildings are predominantly narrow, vertically oriented windows, stacked above each other with regular spacing.
- C.1.2 Openings above the pedestrian level of the street may vary from traditional openings and proportions and may incorporate modern styles and materials.



Proportion of Openings

Proportioning building facades to reflect the scale, variety, and rhythm of the block contributes to Downtown architectural continuity.

C.2 Horizontal Rhythms

Design Objective

New buildings shall fit with the general character and image of the Downtown area. Building “fit” can be achieved by replicating the horizontal rhythms of surrounding buildings.

Discussion

Historically, buildings in Downtown urban areas like Spokane have a distinct horizontal rhythm along the street. This horizontal rhythm is created by the size of the lots, the rhythm of openings (windows and entries) along the street, and the use of common horizontal window lines, building lines and cornices. The rhythm is also reinforced with the use of common building materials and window proportions.

The repetition of these common elements in building facades creates a continuous band along a block. The most common band is the division between the storefronts and upper facades of buildings at a similar height, with a predominance of canopies or awnings extending along several facades, the alignment of windows and windowsills, and the alignment of floor-to-floor building lines. Maintaining this strong horizontal band within the range of human visual perception creates a sense of enclosure.

The horizontal rhythms of building facades reinforce the pedestrian activity at the street level, and help to unify the image and character of each block in the Downtown.

Individual landmark buildings, such as a church or a public institution, can gracefully interrupt this rhythm. However, too many landmark buildings clustered together can disrupt the overall unity of the urban streetscape and can also diminish the distinct character of each individual building.

Guidelines

- C.3.1 The horizontal rhythm of the Downtown street wall can be reinforced in new buildings by using a similar alignment of windowsills, building lines, floor lines, cornices, rooflines, and floor-to-floor spacing along a street block.
- C.3.1 Using building design elements such as cornice lines, ground floor canopies and awnings, overhangs and windowsills helps to maintain a clear visual division in building design between the street level (ground floor retail uses) and upper floors.



Horizontal rhythms are reinforced with similar alignments of floorlines, windowsills, cornices, and rooflines.

C.3 Building Form

Design Objective

Building form should be consistent with the character of Downtown as an urban setting and should reinforce the pedestrian activity at the street level.

Discussion

Historically, buildings in the urban center of Spokane have consisted of rectangular forms over two stories in height, covering entire lots. This building shape creates a regular rhythm of building mass and street wall along the commercial streets. The mass of the buildings is articulated with building details, commercial window displays, and entries at the street level that create an attractive pedestrian-friendly environment along the street.

In recent years, new buildings have increased in size and scale with taller buildings covering larger lot areas, and in some instances even whole blocks. While larger in size and building mass, many of the newer buildings maintain a pedestrian scale through the use of “human scale” building elements at the street level. These elements can include window openings, commercial displays, building entries, ornamentation, awnings and canopies, and articulated wall surfaces.

Creating pedestrian scale buildings, especially at the street level, can reduce the mass of buildings. Pedestrian scale can be created, even on upper floors, with the use of “human scale” design elements. Human scale design elements are details and shapes that are sized to be proportional to the human body.

Guidelines

- C.3.1 Incorporating human scale design elements such as windows, elements indicating floor-to-floor heights, appropriately-scaled building materials, cornice lines, signage, and awnings can help reduce the mass of buildings as experienced at the street level.
- C.3.2 Blank walls on the ground floor of street frontages are unattractive and uninviting. Commercial frontages should feature display windows, entries, and other pedestrian amenities.
- C.3.3 High quality materials and architectural ornamentation at the ground floor/street level of buildings can both accent buildings, and provide visual interest for pedestrians and motorists.



Human scale design of buildings creates a pedestrian-friendly environment at the street level.



Blank walls on the ground floor are unattractive, uninviting, and are not human scale.

C.3.4 The form and mass of buildings in the Downtown consist of rectangular building forms. Curving, undulating or diagonal building forms or elements are inconsistent with traditional Downtown architecture.

C.4 Building Styles

Design Objective

New buildings shall not try to replicate one specific architectural style or create a single architectural theme. All building styles are acceptable in the Downtown if the overall design objective of creating an urban, pedestrian-friendly setting, as outlined in the Downtown Plan and Downtown Spokane Design Guidelines is met.

Discussion

Downtown Spokane has buildings representing several historical periods and reflecting many different architectural styles. The varying architectural styles represent an eclectic collection of buildings. Despite the variety of building styles, a number of consistent site and streetscape characteristics synthesize the character of Downtown as an urban, pedestrian-friendly place. The major design characteristics contributing to this synthesis include: buildings constructed to the edge of sidewalks, a regular rhythm of building openings, commercial activity at the street level, and a consistent building wall along the streets.

The following building styles are presented as an educational resource, to describe the origins and design features of buildings that are apparent in Downtown Spokane. New development Downtown might draw on materials and details reminiscent of these particular styles in order to support continuity in Downtown architecture.

Downtown commercial buildings can be grouped into several major types or styles:

Turn of the Century (late 1800 – early 1900) Commercial Offices Buildings and Warehouses. Early 20th century commercial buildings in Downtown Spokane are typically brick, stucco or stone buildings of Gothic Revival, Victorian, Italianate, and Richardsonian Romanesque styles. Other styles of this era include the Chicago School office building, and some classical revival styles.

Art Deco. In the late 1920s Art Deco, a new style emanating from Europe, made a significant impact on American architecture and is represented in Spokane. The Art Deco, Moderne or Modernistic style features decoration consisting largely of low-relief geometrical designs, often in the form of straight lines, zigzags, chevrons and stylized floral motifs. In Spokane, the Art Deco style produced a number of more ornate tile buildings with accents in terra cotta, glass and vivid colors.

Modern. A number of modern styles emerged in the early 1930s as an outgrowth of the international style of architecture. These modern



Downtown Office Building



The retrofit to this historic building covers fine architectural details that could have been preserved and emphasized .



Art Deco Commercial Building.

styles are based on modern structural principles and materials. Concrete, glass and steel are the most commonly used materials. The international style eliminated nonessential decoration and revealed the skeleton-frame of construction. Ribbon windows were a hallmark of this style, as were corner windows, in which the glass was an important design feature. Strip windows and solid planes helped emphasize the horizontal character of buildings. High-rise buildings were designed as one large office, with floors stacked one on top of the other. Other modern styles of the era include Brutalism modern styles using large masses of concrete.

Modern Corporate Offices. By the late 1960s the international style dominated Downtown office design but soon evolved into a style of economic efficiency. Where the early international style was based on principles of *expressing* functionality, the Modern Corporate design honed this style to a new high. Downtown office buildings became a representation of economic functionality. Built almost strictly of glass and steel, the Modern Corporate Office is devoid of all ornamentation and takes the form of a simple box with ribbon windows.

Post Modern. In the recent past, 1970s to 1990s, the Post Modern style emerged in American architecture. In reaction to the international style's lack of ornamentation, Post Modernism uses ornamentation elements in a whimsical way. Oversized design elements from the past are reinserted into the building façade as "card-board" cutout elements. Use of columns, cornices and oversized parapets became common features. While the facades are often ornate, the interiors of many Post Modern buildings remain simply large flat floor plates.

Many other styles are represented in the Downtown area such as "techno" industrial design styles, free-form organic design styles, and more suburban, national franchise architectural styles. The above list is not intended to be an all-inclusive catalog of architectural design styles. Evaluating site context, architectural styles and character of adjacent properties, can help determine appropriate architectural styles for new buildings.

Guidelines

- C.4.1 All design styles are acceptable for use in the design and construction of new buildings in Downtown Spokane if the design accomplishes the overall objectives of creating an urban, pedestrian-friendly setting that contributes to an active street life.
- C.4.2 Building form, materials, signs, logos, colors and other identifying features can help make national franchise



Art Deco detail



Modern Corporate Office



Post Modern Commercial Office.

buildings compatible with the overall design character of the Downtown.

C.5 Roof Forms

Design Objective

Roof shapes shall reflect the “*urban*” character of Downtown Spokane. Taller buildings are encouraged because they contribute to an attractive and interesting skyline for the Downtown. Variety in roof shapes, building setbacks and building materials are also encouraged.

Discussion

Historically, buildings in Downtown Spokane have had flat roof shapes. Flat roofs with parapets are typical of commercial buildings in urban areas. While some buildings may include unique architectural elements such as towers, spires or special cornice designs, the overwhelming number of buildings in the Downtown have flat roof shapes.

Pitched roof forms, particularly on one-story buildings with hipped roofs, are typically characteristic of “suburban” styles and therefore, generally inconsistent with the urban image of the Downtown area.

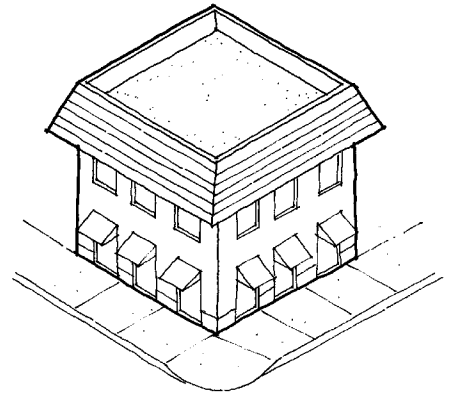
Roof shapes, particularly on taller office buildings, can contribute to creating an interesting and memorable skyline for Downtown Spokane. The building skyline, as seen from a distance, from the Highway and from surrounding hills, creates a physical landmark for Downtown as the center of the community and the center of business, commerce, and government for the region.

Guidelines

- C.5.1 Flat roof forms with parapets are well-suited to the character and image of the urban Downtown environment.
- C.5.2 Special roof shapes on corner locations can be used to help accent corners of blocks.
- C.5.3 Articulated and varied roof shapes on taller office and residential towers add interest to Downtown architecture, and can also serve as Downtown reference points. The use of stepped building setbacks, unique rooftops, and varying building materials can contribute to the penetration of light and the unique skyline of Downtown.
- C.5.4 Roof forms inconsistent with the character of Downtown Spokane include: slope roof shapes on one-story commercial buildings, gable-end roofs, single pitch (shed) roofs, false mansard roofs and curving roofs.
- C.5.5 A visual terminus, such as a heavy cornice, at the tops of buildings helps articulate Downtown architecture.



Varying roof shapes and massing contribute to an attractive skyline.



False mansard roofs are inconsistent with the character of the Downtown area.

D. Landscaping (Hardscape & Vegetation)

D.1 Street Trees

Design Objective

Street trees shall reinforce the 'urban' image character of Downtown Spokane and support the pedestrian friendly character of the street. They shall increase the desirability of pedestrian activity as well enhance the status of street and adjacent property values.

Discussion

The character of the Downtown and the pedestrian environment can be greatly enhanced by the quality and quantity of landscaping and street trees. One of the easiest and least expensive improvements for any urban area is providing street trees along all streets.

Large front lawns are not in keeping with the high intensity urban character of Downtown Spokane. Instead its urban quality needs to be emphasized by a continuous canopy of trees over the sidewalks, and buildings placed at the edge of sidewalks

Not only are street trees attractive, they can improve air quality, add shading, reduce water runoff, and add to the property values of a neighborhood at a relatively low improvement cost. Street trees also create a continuous and regular street tree canopy. They define the pedestrian space along sidewalks, provide separation between pedestrian zone and the travel lanes in the street, provide shading, and generally improve the appearance of streets.

A continuous canopy of street trees in the Downtown will provide, particularly along Type I Green Streets an appropriate pedestrian friendly environment. New development will better fit into the Downtown when it protects existing mature trees on the site and provides new trees of the same variety. Landscaping should blend with adjacent landscaping, reinforce the pedestrian circulation system, direct people to building entrances, provide seasonal color and shade, and conserve water usage.

Street trees in an urban area have to meet special conditions. To both functionally grow in an urban area and contribute to the street environment, urban street trees need to be carefully selected and maintained.

Guidelines

D.1.1 Providing sufficiently spaced large wide canopy trees at regular intervals along the street (20-25 feet) for new

development and renovation projects creates well defined and protected pedestrian sidewalks.

- D.1.2 Selecting street trees that are easy to maintain, helps reduce sidewalk damage
- D.1.3 Pruning street trees to maintain a clear space between the lower branches and the sidewalk and roadway helps prevent damage and also provides views of building signage.

D.2 Landscape Elements

Design Objective

Besides street trees, landscape elements such as planters, shrubs, ground cover, water elements, etc shall create an attractive colorful pedestrian setting in the urban environment of Downtown Spokane. Plant selection should also consider the rate of water consumption.

Discussion

Green is psychologically a restful agreeable color. However the presence of various flowering planter strips, ground covers, etc along a street provides and accentuates the soft, colorful, friendly and enlivening environment needed to contrast the hard physical elements of the urban environment.

Guidelines

- D.2.1 Selectively using high maintenance annuals and perennials creates smaller but cost effective distinguishing landscape accents.
- D.2.2 Selecting plants materials that have low water consumption lowers costs and enhances water conservation.

D.3 Hardscape Elements

Design Objective

Hardcape elements such as pedestrian kiosks, benches, bus shelters, newspaper racks, trash cans and café tables shall furnish the street environment and enhance community livability. They shall increase the opportunities for people to socialize and spend time outdoors along public streets, and thereby enhance the urban character of Downtown Spokane.

Discussion

Large front lawns, landscaped front setbacks and front yards are not in keeping with the high-intensity urban image of Downtown Spokane. Hardscape design elements are important to enhance and accentuate the urban quality of the Downtown.

A downtown that has benches, lighting, well-maintained attractively paved sidewalks and other pedestrian amenities that enhance the pedestrian environment at the street level will add to the urban character of the district.

The best streets encourage participation and invite safe leisurely walking. Besides soft landscape elements such as trees, shrubs, etc, hardscape elements such as benches, pavings, signs, etc make the streets more inviting and user friendly

Again a high quality of workmanship and materials used for the hardscape elements is as important as the presence of them.

Guidelines

D.3.1 Providing hardscape elements including paving materials, pedestrian street furniture, and art along public streets enhances the street environment by creating a pleasant and active place for people to walk, congregate, and interact informally.

D.3.2 Where appropriate, considering additional hardscape elements, such as small entry plazas, seating alcove areas, and other pedestrian amenities in the design of buildings at the street level enriches the pedestrian friendly quality of streets.

D.3.3 Considering opportunities for public art displays along the streets in the site and building design process makes the street experience even more lively and interesting.

D.3.4 Fitting the pattern and texture of ground paving materials in the existing context of the district contributes to the overall theme and quality of materials in the Downtown.

D.3.5 Using hardscape materials that are functional, able to endure weather conditions (rain and snow of the region), solid quality and vandal resistant, yet are attractive and able to fit in with the context of materials in the Downtown will help in making a cost effective, easy to maintain, attractive street environment



Bollards can enhance the safety and pleasantness of the pedestrian environment.

E. Signs

E.1 Building Identification Signs

Design Objective

Signs shall be designed as integral parts of the site and architectural design of proposed projects, rather than as afterthoughts.

Discussion

The objective is not to rewrite the sign ordinance but to give suggestions on how to design signs that enhance the character of existing older buildings and support a pleasant, pedestrian scale environment at the street level.

Attractive, artistic, well-proportioned and thoughtfully located signs will enhance the image of Downtown Spokane.

Signs can either enhance or detract from the attractiveness of a Downtown area. Often, positive shopping experiences in successful, pedestrian-friendly Downtown areas are accompanied by attractive, well-synthesized signage. In general, signs should relate in placement and size to other building elements. They should not obscure building elements such as windows, cornices, or decorative details. Sign materials should complement building façades. Individual shop signs in a single storefront should relate to each other in design, size, color, lettering style, and placement on the building. Franchises and chain stores will adapt their graphics to meet local guidelines and ordinances.

This will contribute to a Downtown that effectively orients visitors, while supporting an attractive, pedestrian-friendly experience.

Guidelines

General Sign Guidelines

- E.1.1 Maintaining a minimum clearance above the public right-of-way for signs that project from buildings helps prevent accidents and enhances pedestrian safety.
- E.1.2 Protruding signs above rooflines, eaves or parapets creates unsightly facades and detracts from the architectural quality of the building.
- E.1.3 Firmly anchoring signs that project from the building, to the building façade with attractive, non-corrosive hardware that

will not damage the façade of the building prevents accidents and enhances pedestrian safety.

- E.1.4 Encouraging the use of darker letters against a lighter background makes the signage more legible for the viewer.
- E.1.5 Encouraging merchants to create their own unique signs, symbolic of their personal business creates a distinct Downtown that effectively orients visitors
- E.1.6 Creating a network of quality, well-designed signs, clearly announcing the type of services/uses offered makes the downtown an attractive friendly experience for the downtown visitor. This experience is further enhanced when building signage, indicating names of businesses and reflect the activities that occur within buildings.

E.2 Flush Mounted Signs

Flush Mounted signs are signboards or individual die-cut letters placed on the face of a building. Usually there is a recess or horizontal molded band on buildings that was designed to accommodate a Flush Mounted sign.

- E.2.1 Sizing signs that fit within the proportions of the building façade so they do not crown the top of a building wall or parapet, maintains the architectural quality of the facade.
- E.2.2 Locating Flush Mounted signs on a historic storefront along a first floor cornice line, above the awning or transom windows maintains the architectural identity of the building.
- E.2.3 Centering signs within storefront bays and not extending beyond the limits of the storefront or over elements such as columns, pilasters or transoms, prevents the signage from being obtrusive to the façade character.
- E.2.4 Encouraging the use of die-cut letter signs made from materials consistent with the district and mounted directly on the building enhances an attractive signage vocabulary for the Downtown.

E.3 Hanging/Blade Signs

Hanging signs mounted on buildings, perpendicular to the sidewalk, are very effective for pedestrians since they are placed near eye level.

- E.3.1 Maintaining a minimum clearance above the sidewalk enhances public safety.
- E.3.2 Designing blade signs with areas of more than three feet (3') makes them highly obtrusive and unsightly.

E.3.3 Directly illuminating blade signs located beneath awnings makes it generally illegible for the user.

E.3.4 Encouraging blade signs that use logos, business icons and symbols, creates a user friendly Downtown experience for visitors.

E.4 Window Signs

Window signs identify the corresponding building uses/activities and preserve a majority of the display area for pedestrian window-shopping.

E.4.1 Exceeding 20% of the total area of the window with signs generally makes the window seem too cluttered for the viewer.

E.4.2 Encouraging window signs that use high quality materials such as paint or gold leaf, or that are etched into the glass creates an attractive and visually pleasing façade for viewer.

E.5 Icon or Graphic Signs

Icon or Graphic signs illustrate by their shape the nature of the business within. They are the easiest signs to read and are very well suited to vehicular as well as pedestrian traffic.

E.5.1 Encouraging graphic imagery with subservient text makes for an attractive and informative visual experience.

E.5.2 The display of icon or graphic signs is a creative and effective means of communicating to visitors, the types of businesses and activities located inside buildings, thereby making Downtown a pleasant and user-friendly experience.

E.6 Lighted Signs

Internally or externally lit signs are appropriate in the Downtown area.

E.6.1 Using internally lit signs of black or dark colored backgrounds with light lettering, or individually illuminated letters helps them to be more legible from a distance and causes less glare.

E.6.2 Containing light within the frame of externally illuminated signs accentuates the message and reduces glare and light pollution.

- E.6.3 Orienting and shielding spotlights such that the source of light is not directly visible focuses the attention of the viewer. Additionally, this orienting of illumination prevents unnecessary discomfort for occupants of adjacent buildings.

E.7 Neon and Bare Bulb Signs

Although Neon and Bare Bulb signs are becoming increasingly rare, they can add to the historic quality and uniqueness of Downtown.

- E.7.1 Continuing the use and maintenance of Neon and Bare Bulb signs helps in maintaining the historic character of Downtown.
- E.7.2 The use of Neon and Bare Bulb signs is in entertainment areas such as in bars, restaurants, dance clubs, and other entertainment-oriented businesses, is consistent with the traditional use of such signs.
- E.7.3 Utilizing the pictorial images related to the particular business makes for an attractive user-friendly experience.

E.8 Signs on Awnings

Painting signs on the valence (vertical flap) of awning is a cheap and simple method of creating signage.

- E.8.1 Individual letters painted directly on the valence of an awning facing the street makes for a distinguished and informative sign.
- E.8.2 Composing the awning text to a total area of more than 10 square feet makes the signage not easily comprehensible for the viewer.
- E.8.3 Limiting signage on the sloping surface of an awning to small graphic symbols or logos unique to a particular business helps prevent the information on the signage from getting too cluttered up for the viewer.

E.9 Directional Signage for Parking Lots

- E.9.1 Containing directional signs marking entries and exits to customer parking lots to no more than one commercial image, logo or message, which is subservient to text identifying “customer parking” makes the directional signage easily comprehensible.

E.9.2 Limiting each driveway to no more than one directional sign, located on private property near sidewalks makes the signage more user friendly.

E.10. Banner Signs

Banner Signs can be used temporarily for special events in the Downtown.

E.10.1. Decorative banner signs can be used to add color and create a festive atmosphere for special events, holidays, and seasonal events.

E.10.2. Banners signs may be attached to light standards or projected from building facades.

E.10.3. Locating banners less than 8 feet from grade is unsafe for the public. Similarly locating banners within one foot of the edge of the curb as projected vertically, avoids damage from tall trucks.

E.10.4. Banner signs should be removed or replaced when they show signs of fading or unattractive wear.

E.11 Sign Types Not in Keeping with Urban Character

- Building signs advertising products and vendors, rather than business types and services.
- Flashing, animated, blinking, rotating, reflecting or revolving signs.
- Electronic reader boards and other similar sign types.
- Changeable copy signs, other than for use with a movie marquee.
- Chalkboards or blackboards, other than for use in a restaurant or on a café menu board.
- Portable signs, such as “A” frame signs.
- New, freestanding commercial signs (directional signs for customer parking, etc.).
- Off-site and general advertising signs and billboards.

- Standard product and logo signs provided by national distributors (merchants are encouraged to create their own unique signs, symbolic of their personal business).
- Advertising signs, other than graphic symbols or logos unique to a business, located on the sloping surface of awnings.
- Signs, other than real estate notices, on vacant or closed buildings.
- Temporary signs and promotional decorations, such as pennants and balloons (with the exception of holiday decorations, which should be removed promptly after a holiday has passed).
- Signs on privately owned benches.
- Signs using florescent material.
- Private signs on public property.

F. Design Details

F.1 Awnings and Canopies

Design Objective

Awnings and canopies shall be encouraged as a high priority aim to create a more pedestrian scale experience along commercial streets at sidewalk level, while providing protection from inclement weather and the sun

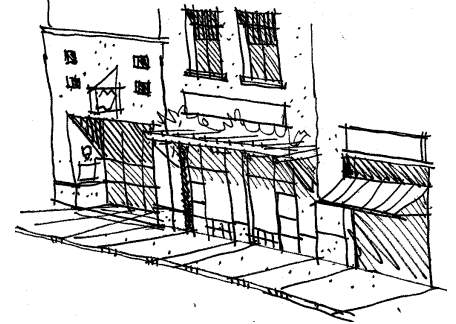
Discussion

Canopies, arcades, awnings and overhangs can provide shade and weather protection and enhance the pedestrian environment along the street at the ground level. They help to define the pedestrian space along commercial building facades. Canopies and awnings can also provide an architectural element in buildings to help articulate the building façade creating greater variety and interest at the street level. Besides being a traditional design element of commercial buildings in Spokane, they can also provide places for signing of commercial uses.

Awnings and canopies come in many shapes, styles and colors. In general, awnings should fit the historic character of the district and building, be well maintained, functional and not obstruct the flow of pedestrian movement along the public sidewalk space.

Guidelines

- F.1.1 Use of canopies, awnings and overhangs throughout the district on ground floor of commercial uses, by locating them over window displays and entries along public sidewalks, creates a pedestrian friendly environment
- F.1.2 Carefully designing awnings in terms of size, shape and placement such that they fit within individual bays or structural divisions of the building façade rather than extending beyond a single bay enhances the architectural quality of the buildings. Conversely, poorly placed awnings can cover historical decorative ornaments, transoms or other architectural elements of the façade.
- F.1.3 Encouraging the use of retractable awnings on darker areas, or north-facing facades of historical storefronts will enhance the climatic environment for the pedestrian.
- F.1.4 Using awnings and canopies over storefronts and entries provide opportunities for colorful accents and signage thereby creating an interesting and active street frontage.



Awnings and canopies provide shade and weather protection and enhance the pedestrian environment.

While a variety of striped and solid colored awnings are available, it is important to remember that canvas awnings often fade over time especially bright-colored awnings, which initially appear to be somewhat intense or bold. Uncolored or light-colored canvas awnings on darker and north facing facades allow daylight to filter through to storefronts and second-story windows.



Glass canopies are an alternative to awnings.

- F.1.5 Using glass canopies, as an alternative to awnings is functional, especially on darker or north-facing building facades. Glass canopies provide rain protection while allowing daylight to filter through to storefronts and second-story windows.
- F.1.6 Awnings and canopies made of canvas, glass, or metal will blend into the character of the Downtown. Conversely vinyl or plastic awnings and canopies often look cheap and flimsy. Similarly striped aluminum awnings and canopies are inappropriate as they are not in keeping with the historic character of the district.
- F.1.7 Fixed awnings or canopies that simulate solid, mansard roof shapes or solid forms can detract from the existing urban design vocabulary of the Downtown.
- F.1.8 Encouraging the use of second and upper floor awnings that complement the ground floor awnings in terms of size, style and color creates a consistent design image for the building façade.

F.2 Use of Color

Design Objective

Appropriate colors shall be used to add to the liveliness of the urban experience and complement the surrounding environment.

Discussion

Use of color is often a sensitive subject in design guidelines. The use of pre-approved colors can lead to a dull streetscape lacking distinction and interest. Matching existing color schemes can also lead to blocks or an entire district in, for example, one variation of brown. In general, the major principle in the selection of building colors is to be a “*good neighbor*.” Colors should coordinate with others buildings on the block or district.

Historically, building colors in Spokane have been associated with the red brick materials used in many of the Downtown buildings. The overuse of dark brown colors, however, can also create a darker image on the street, especially during the winter months, when skies are often overcast. In recent years, new buildings have added more light to Downtown Spokane with the use of lighter colored glass, steel, concrete, and stone materials.

Color selections should also be made with consideration to the orientation of buildings, which can affect the appearance of colors. Colors on south- and west-facing facades will often appear warmer, due to sun exposure, than colors on the north or east sides.

Guidelines

- F.2.1 Using subtle colors, rather than more intense hues on larger surfaces of buildings creates a more pleasing street environment.
- F.2.2 Using a multitude of strong vivid colors buildings, or using colors that are not harmonious with other colors on the building or found on adjacent buildings creates incongruous streetscapes.
- F.2.3 Paint colors related to natural materials used in the building design, such as brick, stone, tiles, and terra cotta can fit in the urban context of Spokane.
- F.2.4 Encouraging the use of contrasting accent colors for architectural details, awnings, and at entrances creates interesting architectural elements.

F.3 Building Materials

Design Objective

Buildings shall use high quality building materials that reinforce the solid and reliable urban image of Spokane and provide a consistent visual relationship within the Downtown district so as to enhance the pedestrian experience at the street level.

Discussion

Building materials add greatly to the overall character and experience of the Downtown. While the structural construction materials may vary, the public face of buildings, or finish materials, should be more consistent. Spokane's buildings, fashioned from the local materials of the Northwest and reflecting local traditions, share a history.

In Downtown Spokane, brick, stone, terra cotta, ceramic tiles, glass and steel are commonly used building materials. These materials provide a strong and consistent image in the urban environment of Spokane.

The quality of building materials varies widely, and it is the quality of the finish materials and its application that contributes to the continuity of the Downtown character and the pedestrian experience at the street. Building materials on the ground floor of buildings are especially important. The ground floor is where most people can easily come into contact with the building's edge, where materials can be touched and easily seen. Quality building materials and their application add texture and richness to the pedestrian environment.

Guidelines

- F.3.1 Continuity and contextual fit can be created by using common materials found in Downtown Spokane, that are similar in quality, character, texture, finish, and dimension to those commonly found in the best-designed buildings in Spokane (such as brick, stone, concrete, masonry, steels, glass, terracotta, and glazed tiles). Use of these materials creates and conveys a sense of stability and strength to the urban environment
- F.3.2 Use of materials such as artificial stone, mirrored glass, untreated wood, diagonal wood, rough-sawn wood and horizontal wood siding on large building surfaces creates an incongruous effect to the urban quality of the built environment. Special details and street level facades using wood and other non-traditional materials can help to identify special uses and activities within the building. For example

Japanese restaurants traditionally use wood and other stucco materials.

- F.3.3 Encouraging the use of highest quality of affordable facing materials, such as stone, masonry, and other highly textured and solid materials, at the pedestrian street level adds to the richness of the pedestrian experience.
- F.3.4 Using mirrored glass on the ground floor of buildings facing pedestrian-oriented streets (Type I and Type II Green Streets) creates unfriendly pedestrian environment and limits the visual access and permeability of the building façade at the street level. Permeable surfaces at the street level (windows, doors and entry features) helps to create a safe and active appearance.

F.4 Service, Loading and Mechanical Equipment

Design Objective

Areas used for service loading and mechanical equipment shall be designed to protect nearby areas from unsightly, noisy, and noxious environments. Rooftop and ground mounted mechanical equipment and trash storage areas shall be screened from view from adjoining properties and public rights-of-way.

Discussion

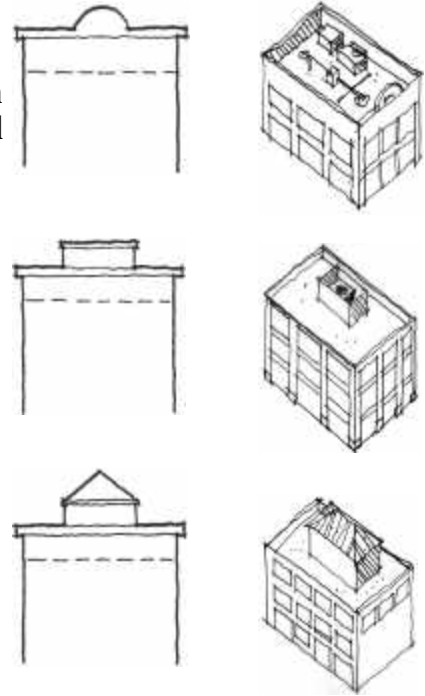
Service areas, loading docks, delivery areas, and mechanical equipment are all necessary functional elements of a downtown. While necessary, these elements often detract from the pedestrian experience and the visual urban environment.

Functional service areas of buildings should receive the same design attention and consideration as more public spaces. Although the materials and finishes need not be the highest quality, functional service requirements of a building should be carefully placed and screened to reduce the visual blight in the urban setting. In many cases, the functional requirements can be placed at the back of buildings, off alleyways and side streets. Screening and enclosures also offer the opportunity to create attractive and interesting design elements to a building project, rather than a purely functional solution.

Guidelines

- F.4.1 Whenever feasible locating loading areas to be accessible from alleyways, side streets, interior parking garages, or from the rear of buildings rather than from the fronts of buildings, creates an attractive and pedestrian friendly Downtown.
- F.4.2 Functionally separating loading areas from parking and pedestrian walkways enhances pedestrian safety and provides convenient access for delivery trucks.
- F.4.3 Locating and designing loading areas to minimize their visibility from public areas and adjacent properties lessens their impact on the downtown image. Similarly erecting substantial and attractively constructed fences or walls to screen dumpsters and trash enclosures, helps prevent the visual blight generally associated with this activity.

F.4.4 Locating mechanical equipment (including air conditioning units, pipes, ducts, vents, access doors, meters, transformers and other building systems equipment), away from pedestrian ways and seating areas helps minimize noise, exhaust or visual unsightliness. Additionally screening or hiding such equipment from public view will help preserve the character of the building architecture and the surrounding district.



Alternatives for roof top screening

F.5 Lighting

The following are general lighting guidelines for Downtown Spokane. The discussion and guidelines below relate to lighting project lighting on buildings, parking and landscaped areas and a site in general. While street lighting design is a major factor contributing to the form and character of the Downtown, the City of Spokane is responsible for the design of street lighting fixtures.

Design Objective

Lighting shall be provided on sites and buildings in the Downtown to improve the safety and security during the evening hours and enhance the character and quality of the Downtown experience as an urban place and center of the community. The form, quantity and character of lighting and the quality of light shall establish an attractive, distinctive and safe environment. Furthermore, lighting shall not create an unwanted nuisance for residential area or other sensitive areas.

Discussion

Lighting on buildings and sites has a dramatic effect on the mood, quality, and character of the Downtown. Lighting also increases the safety and security of a site, the street, and surrounding properties during the evening hours. Lighting effects contribute to the character of a place with the color, amount, intensity, and types of lighting used on buildings and in landscaping features. Lighting can create a soft quite mood, or can help create a lively and festive mood in a place.

Guidelines

General Lighting Guidelines

F.5.1 Providing all lamps with metal halide type light fixtures creates greater color rendering for ease of color recognition and consistent light quality throughout Downtown Spokane. Conversely mercury vapor utility yard lights or other light fixtures with high intensity discharge lamps or bulbs, are not suitable for the district as they are not designed to limit or control light direction and do not shield the light source from view from neighboring residential areas.

F.5.2 Maintaining the same type, color, and family of fixture styles for all lamps fixtures used in parking areas as used in the building fixtures on the same site creates a well consistent image for the Downtown.

- F.5.3 Lighting pedestrian areas with pole or bollard type fixtures (typically not more than 16 feet in height or 3 feet in height for bollards) that are in scale with the pedestrian enhances a pedestrian friendly environment.
- F.5.4 Lighting fixtures shall not produce excessive glare, or trespass into residential areas. On-site lighting shall be designed, installed and maintained to direct light only onto the property on which the light source is located. All lighting fixtures, including spotlights, electrical reflectors and other means of illuminating signs, structures, landscaping, parking, loading and similar areas should be focused, directed and arranged to prevent glare or direct illumination on adjoining properties.
- F.5.5 Performing light level calculations verifies luminance requirements, coverage and intensity. Using maintenance factors in the calculations also verifies that light levels are maintained as lamp and fixture efficiency degrades over time. Similarly considering other factors during all lighting system designs such as disability glares, reflected glare, surface reflectivity, and background lighting helps prevent discomfort to the users.
- F.5.6 Having street lighting pole fixtures attached with house side cutoff shields minimizes light trespass and glare into existing and new residential areas. Additionally, keeping lighting trespass into residential areas from street, sidewalk and building lighting to a minimum prevents discomfort to the residents.
- F.5.7 Attaching appropriate shields to lighting fixtures reduces night sky lighting, and minimizes light and glare illuminating directly up into the night sky.
- F.5.8 Using a minimum number of different lamps and fixtures types for all lighting fixtures minimizes maintenance costs and provides a more consistent and related character to a site. This latter characteristic is further enhanced, when all lighting fixtures and poles having a consistent appearance throughout the site. Where both sidewalks and travel lanes must be illuminated, presence of double-head fixtures along streets produces the required luminance.
- F.5.9 Building mounted cutoff downlight fixtures in combination with cutoff poles fixtures unobtrusively illuminates building service areas without causing glare and light trespass beyond the service area.

- F.5.10 Attaching well-controlled down light fixtures to buildings lights alleyways.
- F.5.11 Illuminating building entries with high amounts of pedestrian activity creates a inviting atmosphere to the user, through the use of lighting effects such as lighting vertical surfaces to crate nighttime focal points; incorporating lighting that illuminate surfaces at entrances, allowing the building interior light to illuminate though glass entry facades, and display windows, or the use of decorative lighting fixtures to announce entries.
- F.5.12 In-door lighting emitted from sidewalk display-windows, building entries and ground floor activity areas contribute to the overall illumination of sidewalk areas, which improves safety and security during the evenings. Even low intensity security lighting on the ground floor of commercial buildings, left on after business hours helps to maintain a well-lighted sidewalk in the downtown.
- F.5.13 Providing power fixtures placed in landscape areas for the use of seasonal decorative lighting accentuates its aesthetic quality.
- F.5.14 Encouraging the use of neon and other specialize lighting effects enhances the attractiveness of the Downtown commercial streets, restaurants and entertainment venues for pedestrian traffic.
- F.5.15 Using decorative up lighting enhances landscape features and building architecture, as long as it does not compete with street lighting and signage.
- F.5.16 Encouraging specialty lighting in trees in outdoor patios and restaurants creates a lively, attractive and festive setting. Seasonal use of white or clear string lighting invokes a similar quality.

Street Lighting

- F.5.17 Maintaining a consistent appearance of all decorative street lighting fixtures, street poles and bases for street poles throughout the Downtown area accentuates its special character. The type character and style of downtown street lighting fixtures is the responsibility of the City of Spokane.
- F.5.18 Using modern or classic historic styles made of metal reflects the important character of Downtown. Selecting designs for streetlight fixtures and streetlight pole to fit the character and

image of the Downtown enhances its quality as an “urban” place. Conversely wood fixtures, “woody” styles or rustic design styles, as well concrete light poles with cobra style lamp fixtures detract its unique Downtown image.

- F.5.19 Designing lighting fixtures styles to fit the urban character of downtown creates a significant character and style as distinctly different from other locations and neighborhoods in the City. Similarly selecting street lighting to provide an appropriate character and lighting coverage distinguishes the Downtown district.
- F.5.20 Designing special styles of street light fixtures and poles to mark special streets in the Downtown distinguishes those streets as special and different from other streets. Special street fixtures further designate the level of “Green Street” (Type I, Type II and Type III).
- F.5.21 Selecting all streetlight fixtures to provide lighting for both the auto and pedestrian way (sidewalks) in the downtown produces a well-lit and safe place.
- F.5.22 To the extent feasible, placing light standards symmetrically along opposite sides of a street produces a pleasing, well lit street.

Building Lighting

- F.5.23 Integrating lighting into the design of wall features presents an attractive façade design. Fixtures attached to the sides or tops of columns using contemporary-style arms, conveys a consistent appearance. Lighting accentuates the rhythm of the wall’s openings and pilasters or texture and materials.
- F.5.24 Relating building lighting to the style and character of lighting on the whole site creates a well-integrated design scheme. A variety of similar styles of fixture design or a similar “family” of fixtures further enhances this quality
- F.5.25 Designing and installing contemporary or traditional metal lighting fixture styles are more in keeping with Spokane urban image and character. Conversely “woody” or rustic style lighting fixtures is not in keeping with the character and image of Downtown Spokane.
- F.5.26 Designing lighting fixtures to fit the character and style of the building enhances the Downtown character.

- F.5.27 Encouraging use of neon signs, bare bulb and other feature lighting for restaurants and entertainment retail activities such as theaters, bars, movies and night clubs is consistent with their character.
- F.5.28 Encouraging special lighting for building features, entries, building towers, architectural ornamentation or pilasters strongly enhances the image of Downtown.
- F.5.29 Attaching lighting fixtures to buildings in appropriate locations prevents them from covering up the major architectural features or architectural ornamentation or historic features.

G. Maintenance

G.1 Maintenance and Materials

Design Objective

Buildings and landscaping in the Downtown shall have ongoing maintenance. Durable materials shall be used; and low maintenance is encouraged.

Discussion

Deferred maintenance often contributes to the shabby appearance of some central business districts. Lack of maintenance and repair may be the major visual problem in the some Downtown districts. Simple cleaning and repair of existing structures and facilities can transform the Downtown, a block or a building overnight.

Guidelines

- G.1.1 Encouraging regular ongoing maintenance of existing buildings and structures like simple repainting, cleaning and replacement of windows, awnings, and landscaping is a more effective and less expensive solution than a major alteration.
- G.1.2 Where possible, using substantial, high-quality materials is preferred over less expensive alternatives that will not endure over time and use, as it makes for an unattractive downtown.
- G.1.3 Using cleaning techniques that do not destroy existing materials such as brick and stone is a sensible solution for otherwise the cost would be very expensive in the long term. For example, sand blasting of brick may destroy the outer surface, causing future water damage to the building surface.
- G.1.4 Maintaining the original surfaces and colors of older buildings rather than painting them helps maintain the original identity of the building. Similarly painting or treating natural or previously unpainted surfaces of brick, ceramic tile, or terra cotta with other coating materials may change the original character of the building.
- G.1.5 Using high-quality materials to replace existing building features (for example, aluminum-framed windows being replaced by wood or steel-framed windows) helps maintain the overall architectural identity of the building. Similarly new windows having same individual details like sash, frame thickness and window depths as the original or historic ones preserves the architectural characteristics of the buildings.
- G.1.6 Considering original color schemes while rehabilitating and maintaining existing buildings preserves its character.

Investigation of the existing colors can be made by carefully scraping away coats of paint in a small area.

- G.1.7 Removing, painting or obscuring existing details and ornaments with new signs, awnings or facade changes sometimes drastically changes the identity of individual buildings.

H. Alterations and New Additions

H.1 Existing Structures and Character

Design Objective

For older buildings with some historic or architectural value, alterations and new additions shall respect the quality and character of the original period and style of the existing structures.

Past alterations that have not been successful in preserving the original architectural character shall be removed.

Discussion

Changes to existing buildings are part of the ongoing evolution of Downtown. The Downtown has developed over time, as illustrated by the variety of architectural styles represented in the existing building stock. As a general principle, new additions and alterations should be designed to respect the original period and style of the building. However, it is not necessary to make a new addition to look “historical” or older than it really is. Creating a “fake” historical building will often cheapen what is truly historic and original in Downtown.

In addition, not all additions or alterations require attention to the existing building style and design. Many existing buildings are of a lesser quality or historic value and complete changes may be a more appropriate design solution. For example, some older buildings, as well as some newer ones, are simple concrete shells with no architectural value or importance. Thus, before each building alteration and addition, it is important to determine the architectural “value” of the existing structure.

Guidelines

- H.1.1 Respecting the original period and style of the existing building while making new additions and alterations would as a generally help preserve the architectural character of Downtown.
- H.1.2 Encouraging the removal of 1950’s and 60’s type façade applications that do not reflect the original style and character of the building architecture helps restore the original architectural image of the Downtown.
- H.1.3 Altering a building to look more “historical” or older than it really is creates an unauthentic ‘fake’ image of the district.

- H.1.4 Encouraging the restoration of the original façade of historically significant buildings, rather than covering the building with a contemporary design, helps in recreating the original architectural character of the building. Additionally basing this restoration on solid historical documentation such as photos or original drawings produces a façade, that is as authentic as possible.
- H.1.5 Preserving the significant historic materials and features helps preserving the historicity of the original Downtown character.
- H.1.6 Avoiding constructing additions on the primary or character-defining elements of the building façade helps maintain the original architectural ethos of the district.
- H.1.7 Minimizing the loss of historic materials on exterior walls helps minimize the loss of original image of Downtown.
- H.1.8 Using building finishes for new additions that are as similar in material, quality, color, and dimension to those used on the existing structure continues the architectural character of the original building.
- H.1.9 Making compatible the scale (size and proportion) of the new addition or alteration with the original building respects the presence of the older building. For example, setting back an additional story on an existing building from the existing roof edge or building façade line ensures that the building's profile is not dramatically changed.
- H.1.10 Creating a visual distinction between the old and the new while making a new addition or alteration distinguishes the historic parts of Downtown. Similarly additions, which are sympathetic to the underlying building style and design, yet being representative of its own time, avoid creating a "phony" historic look.
- H.1.11 Creating a visual separation between the addition and the original structure of an existing building, which is of significant historical value to the community preserves the identity of the original building architecture should be strongly considered.

I. Sidewalk Encroachments

Definitions

1. *Temporary Encroachment* – is a permitted use that is established for no longer than an eight- (8) month duration per calendar year. At the end of the eight-month time frame the use is removed and the area of encroachment is returned to pre-encroachment state.

2. *Permanent Encroachment* – is any encroachment into the public right-of-way that results in an eight- (8) month or longer, duration per calendar year, occupation of public space. A request for a permanent encroachment shall necessitate being processed as a vacation, and thereafter, would not be covered under this section of the Design Review Guidelines. However, should any permanent encroachment, by virtue of the particular circumstances, be permitted, such encroachment shall comply with the guidelines included herein.



CONTEXT

I.1 Responding to Site Characteristics

Design Objective

The proposed encroachment shall take into consideration, and respond to, existing site characteristics.

Discussion

The physical and perceived character of any pedestrian circulation space in the Central Business District or Neighborhood varies greatly. These spaces must necessarily be evaluated on a site by site basis within the context of the characteristics of the site and the adjacent businesses or neighborhood. Sidewalk encroachments that respond to existing site characteristics can contribute significantly to a dynamic and diverse pedestrian environment.



Criteria

- I.1.1 The design of the encroachment should respond in a consistent and positive way to existing topographic conditions. Particular attention should be given the treatment or approach of neighboring buildings or encroachments that have responded to similar topographic conditions on their sites.



Solar access needs too be taken into account

- I.1.2 The design of the proposed encroachment should not impact the solar access to adjacent development and/or public areas, particularly in situations where that development is dependent on, or has incorporated solar access into the design.
- I.1.3 The design of the encroachment should not have a negative impact existing and established street trees and other landscaping in the right-of-way.
- I.1.4 The encroachment structure should be compatible with the existing adjacent and neighboring structures, in relation to bulk, scale, detailing, etc.
- I.1.5 The design should not diminish significant public and private views, where these views have been officially recognized.



Detailing helps establish the character of the street

I.2 Streetscape Compatibility

Design Objective

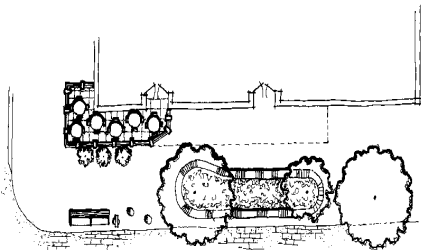
The proposed encroachment shall be compatible with, or complement the existing streetscape.

Discussion

The design and construction of sidewalk encroachments should acknowledge and reinforce the existing desirable spatial characteristics of the right-of-way. The character of our downtown and neighborhoods is often defined by the experience of traveling along its streets. We perceive the quality of these public spaces in much the same way that we evaluate the rooms of a home. How buildings face, are set back from and relate to the furniture of the street and the width of the sidewalk determines the character and proportion of the rooms of our city.

Criteria

- I.2.1 The sidewalk encroachment should architecturally complement the existing street furniture.
- I.2.2 The encroachment should not adversely impact but instead, contribute to the dynamics of pedestrian interaction by providing a stage for action.
- I.2.3 Pedestrian areas should be protected from bicycle and vehicular movement.



Pedestrian areas protected from vehicular movement

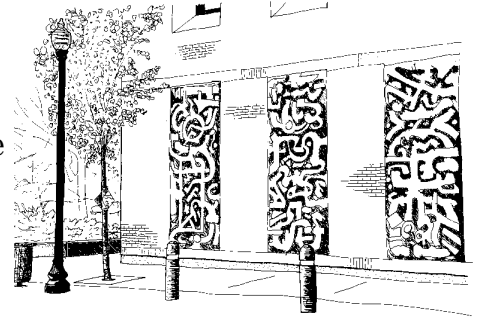
I.3 Architectural Elements and Materials

Design Objective

The proposed encroachment shall be compatible with or complement neighboring developments' architectural elements and materials.

Discussion

Sidewalk encroachments should be compatible with or complement the architectural character and pattern of neighboring buildings. Architectural consistency creates a level of comfort and gives a sense of permanence that encourages human activity while making the declaration that this particular development belongs in this specific location.



Art and street elements add interest to an otherwise blank wall

Criteria

- I.3.1 The sidewalk encroachment should be articulated to create intervals that reflect and promote compatibility with its surroundings. Specifically:
- Modulating, stepping back or extending forward a portion of the facade or barrier.
 - Reflecting the existing building window intervals with the articulation interval.
 - Providing a lighting fixture, trellis, tree, landscape feature or other architectural element within each interval.
- I.3.2 The design of the encroachment should address scale and proportions and considers the characteristics of the adjacent and neighboring buildings.
- I.3.3 The design of the sidewalk encroachment should incorporate an existing or complementary style, or if no consistent style or context exists, that the design complements certain physical conditions of the surrounding development.
- I.3.4 The roof form, if any, should be consistent with, and/or complement the existing context.
- I.3.6 The encroachment should incorporate existing details or patterns. Where specific obvious details or patterns are not included in the design, the reasons for not including them should be indicated.

- I.3.7 Proposed materials should demonstrate a commitment to quality and to a sense of permanence, even though designed for periodic removal.

DESIGN

I.4 Exterior Finish Materials

Design Objective

Exterior building materials that are similar to or compatible with adjacent or neighboring buildings shall be incorporated into the design of the proposed encroachment.

Discussion

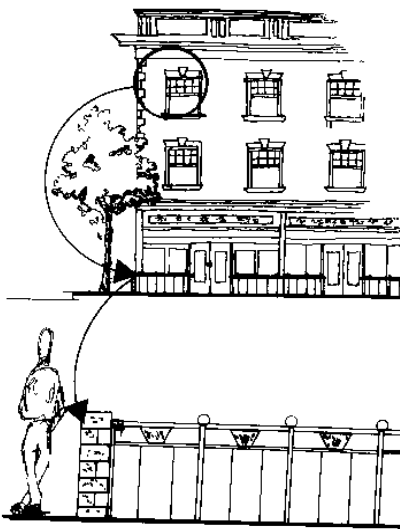
Temporary Sidewalk encroachments should be designed for harmony between the existing building and structures of the encroachment. Compatibility can be viewed in terms of a fit or misfit between the design elements of the project and associated building.

A design that relates to the area's architecture, scale, color, rhythm, and proportion will contribute to the area's character. A design that ignores its neighbors may damage the special qualities and identity of its surroundings.

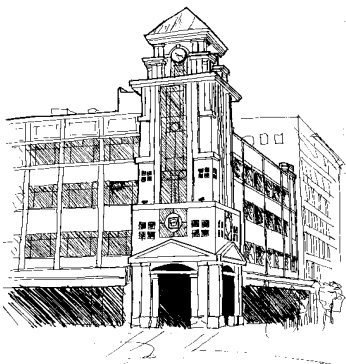
Where departures in form are unavoidable, design features such as texture, materials (wood, iron, Plexiglas), color, details (scroll work, medallions, iron work etc.), columns, pilasters, and proportions should highlight the characteristics of the associated building and surrounding area.

Criteria

- I.4.1 Design compatibility between the encroachment and existing buildings should be achieved by using architectural design and details that add to the identity and character of an area.
- I.4.2 The encroachment should be compatible with the character of the adjacent and neighboring buildings in size, scale, color, texture and materials.



Taking design cues from existing context



Readily apparent street level entrance

I.5 Entrances Visible from the Street

Design Objective

The proposed encroachment shall take into consideration and respond to the location and function of the proposed, the adjacent and the neighboring building entrances.

Discussion

Dynamic interaction between public sidewalks and activities within buildings is critical. Entries to buildings should be clearly identifiable and visible from the sidewalk and street.

Criteria

- I.5.1 The proposed sidewalk encroachment should be designed to reinforce the existing building entryways.
- I.5.2 The encroachment should not adversely impact the interaction between pedestrians and activities within the adjacent or neighboring buildings.
- I.5.3 The proposed sidewalk encroachment should not restrict, or otherwise adversely impact, the use of neighboring building entries.

I.6 Structured Parking Entrances

Design Objective

Vehicular access to parking facilities shall be developed in a manner that places pedestrian safety and comfort as the primary design consideration.

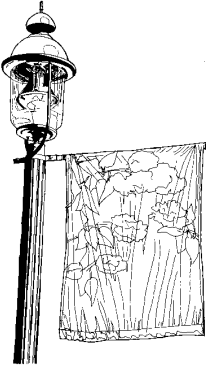
Discussion

Vehicular access across sidewalks is considered a right of property access, even if the access is to a commercial parking lot or structure. The passage of autos to and from these parking facilities is generally a temporary inconvenience for pedestrians. The potential does exist for this interaction between autos and pedestrians to be dangerous and particularly in the case of parking garages, to be intimidating. Pedestrian circulation into and around the site must be given serious consideration to prevent conflicts and conversely, to create an atmosphere that is comfortable for those pedestrians. Safety is, of course, the major design parameter, but features such as: articulation, materials, textures, color, natural light, etc., can be included in the design to allow pedestrians to feel comfortable and that creates a development that is architecturally compatible with the surroundings.

Criteria

- I.6.1 Vehicle access ramps, approaches, and/or aprons where they cross the sidewalks, or other designated pedestrian street crossings should be designed and placed in a manner that is safe for pedestrians.

- I.6.2 Vehicle access should be designed to be unobtrusive and blend-in to the extent possible, with the character of the neighboring development thus creating a pedestrian friendly atmosphere.



Fine textured details create human scale

Pedestrian Environment

I.7 Creating Human Scale

Design Objective

The proposed encroachment shall achieve an appropriate human scale.

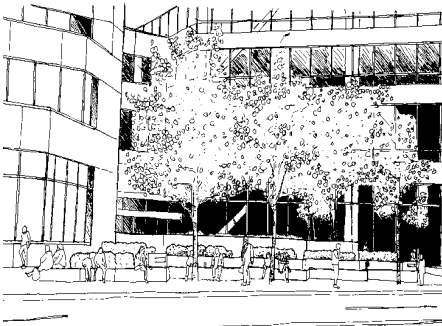
Discussion

The design of sidewalk encroachments should incorporate architectural features, elements and details to achieve an appropriate human scale.

An encroachment reinforces human scale if the details, elements, textures and materials allow people to feel comfortable in using and approaching it. Features that give a building or space human scale encourage human activity.

Criteria

- I.7.1 The design should not include the installation of a narrow barrier or the termination of a vertical surface at eye level, which could create an ambiguous or confusing line of sight.
- I.7.2 The sidewalk restriction cause by the encroachment should still permit the passage of persons (At a rate of a minimum of 10 persons, per minute per foot of walk width).
- I.7.3 The encroachment should be designed and detailed in a manner that encourages human activity and improves the sense of scale.



Vegetation, benches, seating, and fine detailing give human scale to a plaza

I.8 Human Activity

Design Objective

The proposed encroachment shall be developed in a manner that encourages increased human street level activity.

Discussion

Sidewalk encroachments should be sited and designed to encourage human activity on the street. Livelier streets make safer streets. Sidewalk encroachments should integrate activities and elements that encourage and attract activity to the street. These include, but are not limited to:

- Ground floor shops and market spaces.
- Entrances, porches, balconies, decks, seating and other elements that promote use of the street front and interaction among neighbors.
- Drinking fountains, display windows and planter boxes filled with flowering plants.
- Sculpture and art works.

Criteria

- I.8.1 The design of the encroachment should include elements that encourage human activity, while permitting the flow of pedestrians.
- I.8.2 The encroachment should not impede the visual transparency or the perceived physical interaction to activity within the adjacent building's ground floor space.

I.9 Contribute to Urban Activity and Interaction

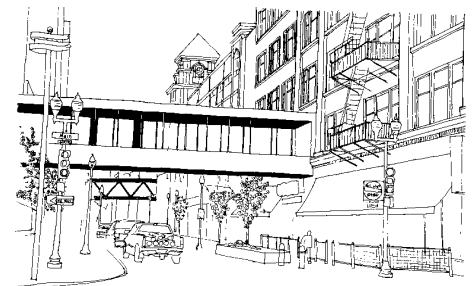
Design Objective

The proposed encroachment shall reinforce the diverse and dynamic experience afforded the pedestrian users of the downtown and neighborhood sidewalks.

Discussion

The improvement of pedestrian circulation space throughout Spokane and especially within in the Central Business District is essential. These spaces are of vital importance because the character of the downtown and neighborhoods is evident when traveling along the sidewalk or street.

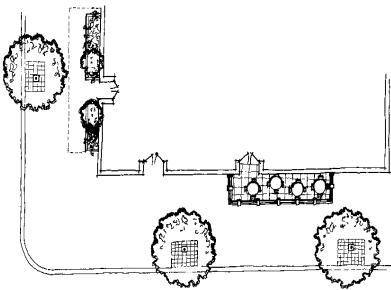
Sidewalk encroachments can contribute to a dynamic and diverse urban environment. Areas which allow or support street activities, such as public artwork, street furniture, sidewalk vending and outdoor cafes create inviting pedestrian settings and encourage contact between the outside and inside of adjacent building and public spaces.



Sidewalk cafes encourage street level activity

Criteria

- I.9.1 The encroachment should contribute to the streetscape by providing nodes of action and for interaction between interior and exterior spaces.
- I.9.2 The encroachment should not inhibit pedestrian circulation in, around or through the sidewalk encroachment.
- I.9.3 Visual and physical access to internal activity spaces from public sidewalks should not be restricted, but should be a design priority.
- I.9.4 The lighting, if intended for use after daylight hours, should be functional, visually distinctive and compatible with the surrounding area.
- I.9.5 The type and placement of plantings should complement pedestrian activities and circulation in and around the encroachment.



Encroachments located to allow adequate pedestrian circulation

I.10 Reinforce and Enhance Pedestrian Circulation

Design Objective

The proposed encroachment shall not be a physical or visual barrier that impedes pedestrian circulation or human activities on the sidewalks.

Discussion

Pedestrian Circulation Spaces are the lifeblood of Spokane's Business District. In order for this lifeblood to remain flowing it is necessary that we eliminate barriers between areas. Physical, and to a large extent visual access binds the diverse sections of the business district together.

Criteria

- I.10.1 An obvious and attractive access route for pedestrian travel should be maintained on the public right-of-way.
- I.10.2 The different zones of a sidewalk: curbs, street furniture zone, walking zone, and window shopping zone should be recognized by, and incorporated into the design.



Attractive pedestrian way

- I.10.3 The sidewalk, walking zone should remain free of barriers for individuals that are physically challenged.

I.11 Provide Public Viewing and Stopping Places

Design Objective

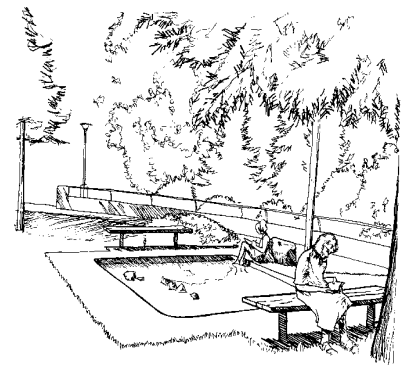
Private encroachments on the right-of-way, when possible and practical, shall include passive use elements that benefit the general public, even though they might not be current patrons of the adjacent development.

Discussion

Sidewalk encroachments can enhance the urban environment by providing drinking fountains, outdoor seating and temporary displays that bring life to the street. People like to sit and watch other people and many prefer to sit where others are sitting, rather than a secluded spot. People-watching, socializing and eating are often combined, providing restful and pleasurable activities for pedestrians.

Criteria

- I.11.1 Safe, comfortable places where people can eat, visit, meet, and rest without conflicting with other street uses, should be provided where practical.
- I.11.2 The passer-by should not be excluded by the erection of unneeded enclosures, or from stopping and sitting to enjoy the downtown.
- I.11.3 The intersection should be recognized as a unified space to be reinforced as an activity area appropriate for seating and other passive activities.



Public seating enhances pedestrian environment

J. Pedestrian Skywalk Design Guidelines

PURPOSE AND INTRODUCTION

The purpose of Spokane's skywalk system is to facilitate pedestrian movement within the core and downtown shopping area. The skywalk system should compliment and not replace pedestrian activity at the street level. Street level pedestrian traffic is essential for a vital and active downtown environment. Skywalks outside of the downtown core are not encouraged, but when permitted, shall comply with the design standards of the Municipal Code and be evaluated by the same criteria as skywalks in the downtown, except for those criteria that relate to being part of the skywalk system. .

Although the skywalk system may be beneficial to downtown, it has the potential to create significant circulation and aesthetic problems. First, the reduction of street level pedestrian circulation can affect activities at the street level. Second, structures built over streets have severe visual impacts on views along the corridors they cross. Finally, the physical character of these structures often conflict with the character of the buildings to which they are attached. Therefore, the expansion of the skywalk system or the construction of individual skywalk bridges, shall be carefully considered and justified, and all components must be adequately addressed, prior to allowing new skywalk bridges to occur.

The streetscapes in downtown Spokane all include buildings that are significant historic resources. Skywalks are a visual intrusion in the character of the streetscapes and should be allowed on new buildings only after careful consideration. Skywalks are not appropriate for historic buildings. Removal of existing skywalks should be encouraged, particularly those connected to historic buildings. The four major components of the skywalk system are: skywalk bridges, skybuildings, skywalks and vertical connectors (see definitions below). For the purpose of these guidelines, the term skywalk bridge will include skybuildings and any appurtenance of the skywalk system built over the right-of-way.

The following policies and guidelines are designed to ensure that any expansion of the skywalk system is designed to minimize the visual and aesthetic impact it may have on its surroundings.

DEFINITIONS

1. **Pedestrian skywalk system** (SMC section 10.02.072)
"Pedestrian skywalk system" means any system of providing for pedestrian traffic circulation, mechanical or otherwise, elevated above ground, within and without the public rights of way, and through or above private property and buildings, and includes overpasses, bridges, passageways, walkways, concourses, hallways, corridors, arcades, courts, plazas, malls, elevators, escalators, heated canopies and access and all fixtures, furniture, signs, equipment, facilities, services and appurtenances. The term includes systems or portions of systems that are built in the future. For purposes of Chapter 10.10, a pedestrian skywalk system includes stairways and escalators leading from or into the skywalk system from private buildings and areas under stairs and escalators leading to and connecting concourse corridors, in addition to stairs and escalators connecting the concourse corridors to public streets or other public property. Ord. C-28629) Passed: 1987/02/23

The skywalk system does not include public pedestrian bridges that solely provide connections to trails or public walks, spanning over or under right-of-ways or the river.

2. **Skywalk System** is the entirety of the Central Business District interconnecting above the street-level pedestrian circulation system. (SMC section 11.04.080 A. 3.)

3. **Major components of the skywalk system** are:

- a. **skywalk bridge** is any elevated bridge-like structure connecting two buildings and primarily designed for pedestrian use. (SMC section 11.04.080 A. 2.).
- b. **skybuildings** are major structures that span streets and provide space for uses in addition to pedestrian circulation, but do contain portions of the skywalk system.
- c. **skywalk area** is those areas within a building through which pedestrians may pass when en route from any skywalk bridge or vertical circulation point to another skywalk bridge or vertical circulation point. (SMC section 11.04.080 A. 1.)
 1. On single-tenant floors this includes the entire floor.
 2. On multi-tenant floors this includes all areas which are not separated from the pedestrian routes by walls, doors, windows, chain gates, rolling grilles, or other devices.
- d. **vertical connectors** are the stairs, elevators, escalators, etc. that connect the skywalk system with the street level.

Exterior Finish Materials

J.1 Disruption of Views

Design Objective

All skywalk bridges shall be built to minimize the disruption of street level views in both directions.

Discussion

Views and vistas are important in creating the character of an urban setting. These can be of significant natural or man made features that provide scale and orientation to pedestrians and motorists alike. Visual termini, often landmark buildings, at the ends of long avenues help create the feeling of a comprehensible and compact urban environment that helps raise the comfort level of the inhabitants. Skywalk bridges can disrupt the visual cues of scale, compactness, and orientation that create the uniqueness of a particular street.

Criteria

- J.1.1 A visual analysis should be used to demonstrate that identified significant views are not seriously affected by the proposed skywalk bridge.
- J.1.2 The use of visual analysis, plans, elevations and /or other visual tools should be employed to ensure the design, lighting, landscaping, or other design elements will minimize the impact of the project on the views from the street or surrounding properties.



Use of compatible colors and materials

J.2 Compatibility with Connected Structures

Design Objective

The connections of any skywalk bridge to their adjacent structures shall be sensitive to the design character of the buildings and be perpendicular to these structures.

Discussion

Skywalks are a relatively new type of construction made possible through the development of modern structural materials. These materials and their construction methods are not always visually compatible with materials and methods employed in the construction of buildings at the turn of the century as is the case with a majority of the downtown buildings. Although the replication of architectural design and elements is not necessary or in many cases even desirable,

efforts should be made to incorporate colors, textures, rhythms, repetitive patterns, shapes, etc. of the connecting building into the design of the skywalk bridge.

Skywalks are not appropriate for historic buildings because they are difficult to integrate into rehabilitation without altering the character of the historic resource. Installation of a skywalk on a historic building will not comply with federal standards used to qualify buildings for local and federal tax credits.

Criteria

- J.2.1 Through the use of plans, elevations, sections and/or other visual tools, it should be clearly demonstrated that the connections to the adjacent buildings do not substantially alter the design character of the building facades.
- J.2.2 The materials used in the construction of the skywalk bridge should compliment the materials used in the adjacent structures.
- J.2.3 The skywalk should not connect to the primary facades of a structure that has been designated as an historic resource.
- J.2.4 The City / County Landmarks Commission should be consulted regarding any skywalk proposed to connect an historic structure.



Detailing and color can be used to increase compatibility

Pedestrian Circulation

J.3 Connections to the Street Level

Design Objective

Each skywalk bridge shall be connected to the street level either at the ends of these structures or within buildings in the blocks to which they are connected.

Discussion

In addition to meeting the minimum number and placement of vertical connections to the skywalk system as required in SMC section 12.02.0474, the access to these connectors shall be inviting, readily apparent, even celebrated in some unique fashion, to encourage shoppers and pedestrians to occasionally change levels on their way from store to store. These vertical connectors should be identifiable from within the skywalk system as well as from street



Identifiable vertical connectors

level so that a locational familiarity and orientation to the street level is achieved.

Criteria

- J.3.1 The skywalk design should enable the pedestrian to reach the street level within the block(s) to which the skywalk connects.
- J.3.2 The vertical connectors should be clearly identified and readily apparent to pedestrians on both the street level and the skywalk level.
- J.3.3 The vertical connectors should be recognized as a major component of the system and emphasized in a manner that encourages vertical circulation.
- J.3.4 The street level points of access should be designed and placed in a manner that provides the pedestrian with street level orientation.



Readily apparent street level access

J.4 Circulation within the Skywalk System

Design Objective

Pedestrian Circulation throughout the system shall be logical and easy to comprehend, with written and visual cues as to location and direction.

Discussion

Circulation throughout the skywalk system must be designed in a manner that does not leave the pedestrian lost or wondering where they are. Confusing passageways, skywalks unconnected to the system, and circulation through spaces such as parking garages, discourages the pedestrian and gives the sense of a haphazardly developed system.

Criteria

- J.4.1 The design of the circulation pathways should be logical, understandable, and connect easily and logically to the existing skywalk system, and is reinforced with easily perceived written or visual cues.
- J.4.2 The design of the circulation pathways that traverse through non-commerce space should make the pedestrian's comfort and ease of circulation the primary design parameters.



Visually understandable walkway and vertical connectors

J.4.3 The design of the skywalk should provide for pedestrian circulation by reinforcing and contributing to adopted goals and objectives regarding pedestrian circulation in adopted plans.

J.4.4 Circulation signage should be provided in accordance with SMC chapter 12.02.0470.

III DESIGN GUIDELINES CHECKLIST

A. Site Design and Layout

A.1 Building Setbacks

Design Objective

Building setbacks help define street and sidewalk areas as active public spaces. Historically, commercial buildings in the Downtown have been built to the front property line, behind the sidewalk. This “street wall” shall be retained and reinforced in new construction and additions.

- A.1.1 Building placement on site
- A.1.2 Placement of recessed entrance
- A.1.3 Historic entryways
- A.1.4 Older buildings with windows

A.2 Outdoor Areas

Design Objective

All site spaces shall be improved for uses and activities to reduce vandalism, increase safety and to provide more attractive and functional spaces.

- A.2.1 Defensible space
- A.2.2 Service access location
- A.2.3 Seating and sidewalk cafes

A.3 Building Heights

Design Objective

The tallest building heights shall be concentrated in the Downtown core with decreasing size and intensity as one moves out, away from the Downtown. Building heights shall be reduced closest to the Spokane River, stepping up in height toward the Downtown core to provide views of River Front Park from buildings farther from the River’s edge.

The Downtown Core, East and West End Districts

- A.3.1 Building height and rooflines
- A.3.2 Existing facades
- A.3.3 Building heights

The South End and Freeway Districts

- A.3.4 Freeway-oriented commercial buildings
- A.3.5 Building heights

A.4 Parking Lot Placement and Design

Design Objective

Parking lots shall be designed so that they are accessible, but do not unnecessarily intrude upon the urban character and the pedestrian quality of the Downtown.

- A.4.1 Parking lot size and location
- A.4.2 Side street and alleyway access
- A.4.3 Landscaping
- A.4.4 Street level commercial activity
- A.4.5 Parking lot screening
- A.4.6 Fences, walls and landscape buffers
- A.4.7 Landscaped berms
- A.4.8 Street wall and pedestrian setting
- A.4.9 Pedestrian amenities
- A.4.10 Signage and location of ingress and egress

B. Transportation and Circulation

B.1 Pedestrian-Oriented Streets

Design Objective

To provide primarily a safe and attractive pedestrian oriented environment for walking, strolling and shopping. Auto traffic on these streets allow for access to adjoining uses and parking.

- B.1.1 Active street edges
- B.1.2 Street furniture
- B.1.3 Physical spatial definition
- B.1.4 Appropriate street proportion

B.2 Pedestrian and Auto Serving Streets

Design Objective

Type II Green Streets shall provide a street experience that is enjoyable to both pedestrian and auto-traffic. They shall also be well connected and easily accessible to surrounding districts.

- B.2.1 Pedestrian comfort and safety
- B.2.2 Convenient pedestrian connections
- B.2.3 Land uses oriented to the street

B.3 Auto-Oriented Boulevards

Design Objective

Type III Green Streets shall provide for a safe and attractive landscaped boulevard environment for moving traffic through

downtown to major destinations. Landscaped boulevards through the downtown shall have a distinctive “urban” character.

- B.3.1 Consolidating entries
- B.3.2 Reduce the number of signs
- B.3.3 Landscaping and street trees
- B.3.4 Building mounted and monument signs
- B.3.5 Attractive street lighting and pedestrian amenities

B.4 Pedestrian Circulation

Design Objective

The design of pedestrian circulation shall create a functional, safe environment for the pedestrian that provides a continuous travel corridor for pedestrians, serving the same major destinations as automobiles. The pedestrian circulation serves local land uses by providing pedestrian access to commercial and residential buildings; serves transit and transit facilities; provides open space and public outdoor activity space to the city supporting social contact and interaction, and provides a buffer from the traffic and noise of the street.

- B.4.1 Adequate width for sidewalk uses
- B.4.2 Mid-block crosswalks
- B.4.3 Decorative paving treatment
- B.4.4 Continuous sidewalk improvements
- B.4.5 Appropriate sidewalk widths
- B.4.6 Bulb-outs at street corners
- B.4.7 Placement of street furniture and street trees
- B.4.8 Separation of sidewalks and bikeways

B.5 On-Street Parking

Design Objective

On-street parking improves the safety of pedestrians on the sidewalk and provides access to adjoining activities and uses. On-street parking shall be designed to be safe and convenient for users and fit the level of activity on adjacent properties and the level of traffic in the street.

- B.5.1 On-street parking as a buffer
- B.5.2 Access to businesses, residential uses, and other land uses
- B.5.3 Angled parking to increase spaces
- B.5.4 On-street parking based on the width of the street
- B.5.5 Different uses of on-street parking lanes

B.6 Bicycles

Design Objective

Accommodations for bicycles and bicycle paths shall be designed to create safe, active and an attractive environment throughout Downtown. The design of bicycle circulation shall create a functional, safe environment for the bicyclist that provides a continuous travel corridor serving the same major destinations as automobiles. The bicycle circulation system shall serve local land uses by providing connections and access to commercial and residential buildings; transit and transit facilities; is part of the City wide open space and public outdoor recreational activity space, and provides an alternative mode of transportation to the automobile.

- B.6.1 Bicycle parking facilities
- B.6.2 Well maintained and clean bicycle lanes
- B.6.3 Direction of bicycle lanes and routes
- B.6.4 Local standards for bike design
- B.6.5 Signage and pavement marking for bike paths
- B.6.6 Avoid streets with diagonal parking

B.7 Gateways and Entryways

Design Objective

Gateways and entryways shall be designed to announce the transition and arrival into Downtown Spokane. These visual features are civic in emphasis and serve to identify and promote the distinct identity of Downtown Spokane.

- B.7.1 Appropriate location
- B.7.2 Civic emphasis and absent commercial names
- B.7.3 Cohesive and integrated
- B.7.4 Illuminated and visible

C. Building Design

C.1 Proportion of Openings

Design Objective

The proportion and rhythm of building openings shall be maintained to create a consistent urban image and character in Downtown Spokane.

- C.1.1 Building openings
- C.1.2 Modern styles and materials

C.2 Horizontal Rhythms

Design Objective

New buildings shall fit with the general character and image of the Downtown area. Building “fit” can be achieved by replicating the horizontal rhythms of surrounding buildings.

- C.2.1 Horizontal rhythm of building elements
- C.2.2 Define street level uses

C.3 Building Form

Design Objective

Building form should be consistent with the character of Downtown as an urban setting and should reinforce the pedestrian activity at the street level.

- C.3.1 Human scale elements
- C.3.2 Blank Walls
- C.3.3 Materials and architectural ornamentation
- C.3.4 Curving, Undulating or diagonal building forms

C.4 Building Styles

Design Objective

New buildings shall not try to replicate one specific architectural style or create a single architectural theme. All building styles are acceptable in the Downtown if the overall design objective of creating an urban, pedestrian-friendly setting, as outlined in the Downtown Plan and Downtown Spokane Design Guidelines is met.

- C.4.1 Design styles
- C.4.2 Corporate image design

C.5 Roof Forms

Design Objective

Roof shapes shall reflect the “*urban*” character of Downtown Spokane. Taller buildings are encouraged because they contribute to an attractive and interesting skyline for the Downtown. Variety in roof shapes, building setbacks and building materials are also encouraged.

- C.5.1 Flat roof forms
- C.5.2 Special roof shapes on corner locations
- C.5.3 Articulated roof shapes
- C.5.4 Slope roof shapes on one-story buildings
- C.5.5 Visual terminus at building top

D. Landscaping (Hardscape and Vegetation)

D.1 Street Trees

Design Objective

Street trees shall be provided in the Downtown area to create a continuous and regular street tree canopy, to help define the pedestrian space along sidewalks, to provide separation between pedestrian zone and the travel lanes in the street, to provide shading, and to generally improve the appearance of Downtown streets.

The landscape treatment should reinforce the *urban* quality and character of Downtown with street trees located in the sidewalk planter strips, use of tree grates and other hardscape materials to create a pleasant and active pedestrian environment along the street edge.

- D.1.1 Street trees requirement and spacing
- D.1.2 Street trees species and type
- D.1.3 Street tree maintenance

D.2 Landscape Elements

Design Objective

Besides street trees, landscape elements such as planters, shrubs, ground cover, water elements, etc shall create an attractive colorful pedestrian setting in the urban environment of Downtown Spokane. Plant selection should also consider the rate of water consumption.

- D.2.1 Maintenance level of annuals and perennials
- D.2.2 Water consumption

D.3 Hardscape Elements

Design Objective

Hardscape elements such as pedestrian kiosks, benches, bus shelters, newspaper racks, trash cans and café tables shall furnish the street environment and enhance community livability. They shall increase the opportunities for people to socialize and spend time outdoors along public streets, and thereby enhance the urban character of Downtown Spokane.

- D.3.1 Hardscape features
- D.3.2 Street level pedestrian amenities
- D.3.3 Public art displays
- D.3.4 Ground paving materials
- D.3.5 Hardscape material durability

E. Signs

E.1 Building Identification Signs

Design Objective

Signs shall be designed as integral parts of the site and architectural design of proposed projects, rather than as afterthoughts.

General Sign Guidelines

- E.1.1 Minimum clearance
- E.1.2 Protrusion above rooflines
- E.1.3 Projecting sign anchorage
- E.1.4 Sign legibility
- E.1.5 Unique symbolic signs
- E.1.6 Clear, well-designed signs

E.2 Flush Mounted Signs

- E.2.1 Sized for building proportions
- E.2.2 Location on historic storefront
- E.2.3 Preservation of storefront elements
- E.2.4 Die-cut letter signs and materials

E.3 Hanging Signs

- E.3.1 Minimum clearance
- E.3.2 Signs area
- E.3.3 Direct illumination
- E.3.4 Logos and business icons

E.4 Window Signs

- E.4.1 Sign area
- E.4.2 Sign materials

E.5 Icon or Graphic Signs

- E.5.1 Communicating type of business
- E.5.2 Graphic imagery

E.6 Lighted Signs

- E.6.1 Internally lit sign lettering
- E.6.2 Illuminated area
- E.6.3 Spotlighted area

E.7 Neon and Bare Bulb Signs

- E.7.1 Use and maintenance
- E.7.2 Entertainment-oriented business

- E.7.3 Pictorial images

E.8 Signs on Awnings

- E.8.1 Painted on the fabric, and size
- E.8.2 Sign area
- E.8.3 Graphic symbols or logos on sloping surface

E.9 Directional Signage for Parking Lots

- E.9.1 Limitation on text
- E.9.2 Number and location

E.10 Banner Signs

- E.10.1 Adding color for events
- E.10.2 Installation and attachment
- E.10.3 Location
- E.10.4 Replacement

E.11 Sign Types not in Keeping with Urban Character

- Product advertising
- Flashing or moving
- Electronic reader boards
- Changeable copy
- Chalkboards
- Portable
- Freestanding
- Off-site advertising
- National distributors
- Advertising on the sloping surface of awnings
- On vacant or closed buildings
- Temporary and promotional
- On privately owned benches
- Florescent
- On public property

F. Design Details

F.1 Awnings and Canopies

Design Objective

Awnings and canopies shall be encouraged as a high priority to create a more pedestrian scale experience along commercial streets at

sidewalk level, while providing protection from inclement weather and the sun

- F.1.1 Over window displays and entries
- F.1.2 Size, shape and placement
- F.1.3 Retractable awnings
- F.1.4 Use of color
- F.1.5 Glass canopies
- F.1.6 Vinyl, plastic and striped aluminum awnings
- F.1.7 Mansard roof shapes discouraged
- F.1.8 Second and upper floor architectural projections

F.2 Use of Color

Design Objective

Appropriate colors shall be used to add to the liveliness of the urban experience and complement the surrounding environment.

- F.2.1 Building colors on large surfaces
- F.2.2 Multiple vivid colors
- F.2.3 Colors related to natural materials
- F.2.4 Contrasting accent colors

F.3 Building Materials

Design Objective

High quality building materials that reinforce the solid and reliable urban image of Spokane and provide a consistent visual relationship within the Downtown Districts shall be used to enhance the pedestrian experience at the street level.

- F.3.1 Use of common materials
- F.3.2 Artificial stone, untreated wood, or mirrored glass
- F.3.3 Stone, masonry, and highly textured, solid materials
- F.3.4 Mirrored glass

F.4 Service, Loading and Mechanical Equipment

Design Objective

Areas used for service loading and mechanical equipment shall be designed to protect nearby areas from unsightly, noisy, and noxious environments. Rooftop and ground mounted mechanical equipment and trash storage areas shall be screened from view from adjoining properties and public rights-of-way.

- F.4.1 Separation for safety
- F.4.2 Visibility and screening
- F.4.3 Screening of mechanical equipment

F.5 Lighting

Design Objective

Lighting shall be provided on sites and buildings in the Downtown to improve the safety and security during the evening hours and enhance the character and quality of the Downtown experience as an urban place and center of the community. The form, quantity and character of lighting and the quality of light shall establish an attractive, distinctive and safe environment. Furthermore, lighting shall not create an unwanted nuisance for residential area or other sensitive areas.

- F.5.1 Metal halide type light fixtures
- F.5.2 Consistent type, color and style of lamp fixtures
- F.5.3 Pole or bollard type fixtures for pedestrian areas
- F.5.4 No excessive glare
- F.5.5 Light level calculations and maintenance factors
- F.5.6 Minimal trespass and glare in residential areas
- F.5.7 Appropriate shields
- F.5.8 Minimum number of lamps and fixture types
- F.5.9 Cutoff downlight fixtures with cutoff pole fixtures
- F.5.10 Well controlled downlight fixtures
- F.5.11 Illuminated building entries
- F.5.12 Well lighted sidewalk areas
- F.5.13 Power fixtures for seasonal decorative lighting
- F.5.14 Neon and other specialized lighting effects
- F.5.15 Decorative up lighting
- F.5.16 Specialty lighting in trees
- F.5.17 Consistent appearance
- F.5.18 Modern or classic historical style
- F.5.19 Urban character
- F.5.20 Special styles to distinguish special streets
- F.5.21 Well lit auto and pedestrian ways
- F.5.22 Symmetrical light standards
- F.5.23 Integrated into design of wall features
- F.5.24 Related to style and character of site
- F.5.25 Contemporary or traditional metal fixtures
- F.5.26 Character and style of building
- F.5.27 Neon signs, bare bulb, and other feature lighting
- F.5.28 Special Lighting
- F.5.29 Appropriate placement of lighting fixtures

G. Maintenance

Design Objective

Buildings and landscaping in the Downtown shall have ongoing maintenance. Durable materials shall be used; and low maintenance is encouraged.

- G.1.1 Maintenance of existing buildings
- G.1.2 High-quality, low maintenance materials
- G.1.3 Cleaning techniques
- G.1.4 Original surfaces and colors
- G.1.5 In replacing existing building features
- G.1.6 Original color schemes
- G.1.7 Existing details and ornaments

H. Alterations and New Additions

Design Objective

For older buildings with some historic or architectural value, alterations and new additions shall respect the quality and character of the original period and style of the existing structures. Past alterations that have not been successful in preserving the original architectural character shall be removed.

- H.1.1 Original building period and style
- H.1.2 Removal of subsequent façade applications
- H.1.3 Remodeled “historical” look
- H.1.4 Restoration of original façade
- H.1.5 Historic materials and features
- H.1.6 Primary façade and character-defining elements
- H.1.7 Loss of historic materials
- H.1.8 Materials for additions and alterations
- H.1.9 Existing building profile and scale
- H.1.10 Visual distinction between old and new
- H.1.11 Preserving identity of original building

I. Sidewalk Encroachments

I.1 Responding to Site

Characteristics

Design Objective

The proposed encroachment shall take into consideration, and respond to, existing site characteristics.

- I.1.1 Existing topographic conditions
- I.1.2 Solar access for adjacent development
- I.1.3 existing and established street trees
- I.1.4 compatible with the existing structures
- I.1.5 officially recognized public and private views

I.2 Streetscape Compatibility

Design Objective

The proposed encroachment shall be compatible with, or complement the existing streetscape.

- I.2.1 existing street furniture
- I.2.2 contribute to pedestrian interaction
- I.2.3 Pedestrian areas protected

I.3 Architectural Elements and Materials

Design Objective

The proposed encroachment shall be compatible with or complement neighboring developments' architectural elements and materials.

- I.3.1 compatibility with its surroundings
- I.3.2 characteristics of adjacent and neighboring buildings
- I.3.3 existing or complementary style
- I.3.4 roof form
- I.3.6 incorporate existing details or patterns
- I.3.7 quality materials giving a sense of permanence

I.4 Exterior Finish Materials

Design Objective

Exterior building materials that are similar to or compatible with adjacent or neighboring buildings shall be incorporated into the design of the proposed encroachment.

- I.4.1 architectural design and details
- I.4.2 character of adjacent and neighboring buildings

I.5 Entrances Visible from the Street

Design Objective

The proposed encroachment shall take into consideration and respond to the location and function of the proposed, the adjacent and the neighboring building entrances.

- I.5.1 existing building entryways
- I.5.2 impact on pedestrians and activities
- I.5.3 impact on the use of neighboring building entries

I.6 Structured Parking Entrances

Design Objective

Vehicular access to parking facilities shall be developed in a manner that places pedestrian safety and comfort as the primary design consideration.

- I.6.1 Vehicle access ramps, approaches, and/or aprons

- I.6.2 character of the neighboring development

I.7 Creating Human Scale

Design Objective

The proposed encroachment shall achieve an appropriate human scale.

- I.7.1 creation of an ambiguous or confusing line of sight
- I.7.2 restriction on the passage of persons
- I.7.3 encourage human activity and sense of scale

I.8 Human Activity

Design Objective

The proposed encroachment shall be developed in a manner that encourages increased human street level activity.

- I.8.1 elements that encourage human activity
- I.8.2 visual transparency and interaction to activity

I.9 Contribute to Urban Activity and Interaction

Design Objective

The proposed encroachment shall reinforce the diverse and dynamic experience afforded the pedestrian users of the downtown and neighborhood sidewalks.

- I.9.1 provide nodes of action
- I.9.2 inhibit pedestrian circulation
- I.9.3 Visual and physical access to internal activity spaces
- I.9.4 lighting design
- I.9.5 type and placement of plantings

I.10 Reinforce and Enhance Pedestrian Circulation

Design Objective

The proposed encroachment shall not be a physical or visual barrier that impedes pedestrian circulation or human activities on the sidewalks.

- I.10.1 obvious and attractive pedestrian route
- I.10.2 incorporation of different zones of a sidewalk
- I.10.3 barriers for physically challenged individuals

I.11 Provide Public Viewing and Stopping Places

Design Objective

Private encroachments on the right-of-way, when possible and practical, shall include passive use elements that benefit the general public, even though they might not be current patrons of the adjacent development.

- I.11.1 Safe, comfortable places for people
- I.11.2 exclusion of passer-bys
- I.11.3 reinforcing the intersection

J. Pedestrian Skywalk Design Guidelines

J.1 Disruption of Views

Design Objective

All skywalk bridges shall be built to minimize the disruption of street level views in both directions.

- J.1.1 visual analysis for views
- J.1.2 visual analysis from the street or surrounding properties.

J.2 Compatibility with Connected Structures

Design Objective

The connections of any skywalk bridge to their adjacent structures shall be sensitive to the design character of the buildings and be perpendicular to these structures.

- J.2.1 connections to adjacent buildings
- J.2.2 complimentary materials
- J.2.3 primary facades of an historic structure
- J.2.4 The City / County Landmarks Commission

J.3 Connections to the Street Level

Design Objective

Each skywalk bridge shall be connected to the street level either at the ends of these structures or within buildings in the blocks to which they are connected.

- J.3.1 street level access
- J.3.2 clearly identified vertical connectors
- J.3.3 vertical connectors to encourage vertical circulation
- J.3.4 street level orientation

J.4 Circulation within the Skywalk System

Design Objective

Pedestrian Circulation throughout the system shall be logical and easy to comprehend, with written and visual cues as to location and direction.

- J.4.1 connect easily and logically to the existing skywalk system
- J.4.2 traversing through non-commerce space
- J.4.3 adopted goals and objectives in adopted plans
- J.4.4 circulation signage

IV Appendices

A. The Secretary of the Interior's Standards for Rehabilitation

Introduction

The Secretary of the Interior's Standards for Preservation, Rehabilitation, and Restoration are not adopted as part of the Downtown Spokane Design Guidelines document. They are reference in the bibliography and should be consulted when changes or additions are being made to historic structures.

"Rehabilitation" is defined as "the process of returning a property to a state of utility, through repair or alteration, which makes possible an efficient contemporary use while preserving those portions and features of the property which are significant to its historic, architectural, and cultural values."

Initially, the guidelines were developed by the Secretary of the Interior to determine the appropriateness of proposed project work on registered properties within the Historic Preservation Fund grant-in-aid-program. The Standards for Rehabilitation have been widely used over the years, particularly to determine if rehabilitation qualifies as Certified Rehabilitation for Federal tax purposes. In addition, the Standards have guided Federal agencies in carrying out their historic preservation responsibilities for properties in Federal ownership or control; and for State and local officials in reviewing both Federal and nonfederal rehabilitation proposals. They have also been adopted by many historic districts and planning commissions across the county.

Intent

The intent of these Standards is to assist the long-term preservation of a property's significance through the preservation of historic materials, and features. The Standards pertain to historic buildings of all materials, construction types, sizes, and occupancy. They apply to both the exterior and interior of the building as well as to related landscape features of the site and more general environment. Finally, the Standards also apply to attached, adjacent, or related new construction. To be certified for Federal tax purposes, a rehabilitation project must be determined by the Secretary to be consistent with the historic character of other structure(s), and where applicable, the district in which it is located.

The Standards

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.

2. The Historic character of a property shall be retained and preserved. The removal of distinctive material or alterations of features, spaces, and spatial relationships that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, shall not be undertaken.
4. Changes to a property that have acquired historic significance in their own right shall be retained and preserved.
5. Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and where possible, materials. Replacement of missing features shall be substantiated by documentary and physical evidence.
7. Chemical or physical treatments, if appropriate, shall be undertaken using the gentlest means possible. Treatments that cause damage to historic materials shall not be used.
8. Archeological resources shall be protected and preserved in place. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and shall be compatible with the historic materials, features, size, scale, and proportion, and massing to protect the integrity of the property and its environment.
10. New additions and adjacent or related new construction shall be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

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